

# CITY OF MOLALLA PARKS, RECREATION AND TRAILS MASTER PLAN



## APPENDICES

## Appendix A

# City of Molalla Parks, Recreation and Trails Master Plan

PROJECT ADVISORY COMMITTEE (PAC)

MEETING #1

February 5, 2007, 7-9 p.m.

### **In Attendance:**

#### *Committee Members*

Glen Boreth  
Mike Clarke  
Paul Ericksen  
Danna Jacober  
Wayne Koster  
Richard Miller  
Maureen Phillips  
Jim Taylor  
Robert Trexler  
Randy Williams

#### *Staff and Consultants*

Mike Clarke, City of Molalla  
Ellie Fiore, Cogan Owens Cogan  
Matt Hastie, Cogan Owens Cogan  
Eldon Lampson, City of Molalla  
Shane Potter, City of Molalla

#### *Community Members*

Wayne Dettwyler  
Steve Courtrain

### **Welcome and Introductions**

Shane welcomed the group and thanked them for agreeing to serve on the committee. Next, meeting attendees introduced themselves and Matt provided an overview on the agenda.

## **Meeting Schedules**

The group agreed to continue to meet on Mondays from 7-9. There is a public meeting scheduled for March 1<sup>st</sup>. The **next PAC meeting** will be on **Monday, March 12<sup>th</sup>**.

## **Project Background, Overview and Schedule**

One of the first steps for this project was to develop a public involvement plan, which outlines how we will inform Molalla residents about the project and get their input. The PAC is one of these tools. We also will post flyers, use the city Web site for meeting notices, coordinate with the Molalla Pioneer and send questionnaires out through city-wide mailings (with the city water bill). We also have been collecting and reviewing materials from the city on parks and recreation.

## **Level of Service Standards**

Communities use these standards to evaluate the function or performance of their parks and recreation facilities. They typically assess the number of acres, parks, types or facilities in a community in comparison to the number of people who live there or in proximity to residents. Public input, evaluations of existing facilities, national guidelines and other communities' standards generally are used to develop level of service standards. The standards are tailored to the local community.

Establishing level of service standards is an early step in the master planning process. It helps to identify both strengths and shortcomings in existing park and recreation facilities and to identify community needs. The master plan will identify strategies to meet these needs, set priorities and identify funding options. Each of these elements will be combined into the draft master plan. This plan will be reviewed with the public and then ultimately sent to the planning commission and city council for review and adoption.

## **Committee Roles and Responsibilities**

The biggest expectation of committee members is that they regularly attend PAC meetings and comment on draft work products. We will hold three meetings over the course of the project which are important opportunities to get PAC member input. Another important function of the PAC is to help get the word out about this process and encourage public participation.

During the next two meetings, we will review the preliminary needs assessment, trails planning recommendations, cost estimates, a strategic plan, and the draft master plan. Comments will be solicited throughout the process. The PAC is encouraged to attend the public meetings and presentations to council and planning commission.

## **Priority Needs**

Matt reviewed input heard at the kick-off meeting with the city's Planning Commission and staff. The PAC discussed and added to these items.

### *Trails*

Matt identified the following priorities.

- Incorporate equestrian use
- Good organizing element for the park and recreation system as a whole
- Potential loop trail around/outside of town
- Use of rail corridor as a trail
- Bear Creek trail
- Trail connection to Elk Park

Committee members noted the following comments and questions.

- Will the trail system connect our existing parks? This is an important feature of a potential trails system.
- Linking to large, new subdivisions is important. There is an existing problem with insufficient parks in the city. Large subdivisions are making the problem worse because they are far from existing parks and do not always incorporate parks within them. Providing parks in subdivisions with links between them would be desirable.
- Equestrian use is important because of how many horses the community has. Molalla Forest Road is a good use to that.
- Shane noted that Molalla Forest Road may likely become a highway if plans for a bypass project go forward. We need to take this into account and plan to incorporate and/or find a parallel trail for the future.
- A Bear Creek linkage would be good as well.
- Enabling trail users to access parks relatively easily will facilitate the use of those parks.

- The safety of trails is important, especially with many children in subdivisions. This also may argue for locating neighborhood parks relatively close to homes on average.
- Trails should accommodate pedestrians, bikes and equestrians.

### *Neighborhood and Community Parks*

Staff and Planning Commissioners have noted a significant need for neighborhood parks, including within subdivisions. One of our tasks will be to identify appropriate standards for neighborhood parks in terms of average size, amenities and proximity to residents. We also will create maps that illustrate where parks currently are lacking.

Neighborhood parks typically have some sort of play structure, picnic facilities, basic playing fields, paths and sometimes shelters. Community parks have multiple playing fields and larger gathering areas. They are larger and draw visitors from a broader geographic area. Some communities create mini-parks which include benches, a small green space and a play structure. Mini-parks are more expensive to maintain per acre and can make it more difficult to provide other types of amenities. Matt asked PAC members to think about what types of amenities should be included in neighborhood parks and if there should be more emphasis on neighborhood parks (2-5 acres) or larger community facilities.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- Neighborhood parks should include restrooms and playgrounds.
- There are financial challenges to creating numerous neighborhood parks.
- Half a mile seems quite far to walk to a neighborhood park, especially for children.
- We should get input from contractors or builders.
- If school facilities, playing fields and open spaces are counted, the city may be pretty close to meeting its current acreage standard for parks.
- There is a lot of existing open space right now.
- The northern section of the city is lacking in parks.
- The city may be meeting standards regarding acreage, but there is a lack of facilities.
- Will you provide a cost analysis for the size of parks, including maintenance and acquisition.

- *We can provide some basic information on this. We'll look at what Molalla is doing now, and compare it to costs in other communities.*
- Consider using homeowners' associations to maintain parks as well.
- The city has some nice existing parks and city has done a pretty good job of creating and maintaining them. However, existing parks are small, and recreational facilities within them are limited. Schools are a good resource and there are other opportunities for sports as well. However, there are not enough parks in the northwest corner of the city along and north of Toliver Road. Future growth and UGB expansions are likely to take place in that area. There is a need for a larger recreational facility there as well.
- The abandoned railroad is an ideal link from the southern to northern areas of the city.
- *Through this master planning process, we can begin to identify tools or strategies to acquire land. Identifying opportunity sites and funding mechanisms is also part of the process.*
- With a growing community, parks should be spread out so that families don't have to travel far to reach them.

### *School Facilities*

At the kick-off meeting, it was noted that playing fields are usually associated with school districts. The school district allows for some community use of these facilities, although schools have the first preference for them.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- There is a general shortage of playing fields. (agreement among committee members)
- School fields are usually available for community use when not reserved for school use. There is no inter-governmental agreement (IGA) or formal contract for community use of school field. The school district currently charges for use of the football field.
- There is constant demand for grass fields which don't hold up well under heavy levels of use. Community use of these facilities worsens conditions for high school use. The school district can't afford to maintain quality of grass fields and spaces with current cost of water. It is an issue of finances and capacity. The school district is open to continued community use of its fields but will need to develop some type of cost or maintenance-sharing agreement to continue to do that.

### *Other Facilities and Topics*

Matt asked the group what else is missing from the list of important issues and needs.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- More skatepark elements around the city, including in neighborhood parks.
- Eldon added that more education around skateparks is needed. It makes sense to move away from skate parks towards smaller skate facilities that cater to younger riders.
- Splash parks (like Clark Park) are popular. They can provide clean water and a good environment and can be relatively modest in size.
- Small recreational amenities at neighborhood parks.
- Open dog parks and off-leash areas for dogs should be considered or discussed.
- *The group should look at different aspects of recreation. The city is recommending an aggressive standard of acreage per resident.*
- There is need for a community center that offers indoor recreational opportunities, meeting spaces and other amenities. (general agreement by the group)
- We need to mandate system development charges (SDC's).
- The plan needs to be affordable and the group should be realistic about what is proposed.
- *It is important to create a plan that can be implemented with existing and expected city resources.*
- The city should not overwhelm any one facility, including schools and the Buckaroo stadium. It is critical not to impede the ongoing function of the Buckaroo. The stadium needs all of its parking available to reach its full market potential for specific events and times of the year.
- *This is a common concern in many communities in which we have worked. Shared or joint use is often problematic. On the other hand, park and recreation facilities are expensive to build and maintain, particularly if a city expects to have a stand-alone system without sharing use of facilities owned by other community groups or agencies. Arrangements for sharing are common and beneficial. Formal agreements can help clarify parameters for shared use and cost or maintenance sharing practices.*

- *The city is committed to working with Buckaroo representatives regarding possible proposals for shared use of that facility.*

### **Level of Service Standards**

Proposed city standards (in the city's draft updated Comprehensive Plan) are 12.5 acres of parks and open space per 1,000 residents, including 10 acres of programmed recreational or park space and 2.5 acres of passive open space. The consulting team will prepare a map that shows existing parks with distance standards. We will bring this map to the public meeting and next PAC meeting.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- Reaching that standard may be more than we can afford in the short-term. Are we meeting that standard now?
- *The city may actually be above this standard, particularly if we include Ivor-Davies Park.*
- The city doesn't want to take care of all parks and probably can't afford to. We should encourage or require home owner associations to maintain some neighborhood parks.
- *That may be one alternative approach to consider, along with other community partnerships.*
- The city is currently charging the maximum SDC's allowed under current state law.
- *SDCs are based on current levels of service. They can be adjusted periodically to respond to growth, construction costs, and other factors. SDCs can only fund acquisition and development of new facilities, along with planning for them.*
- What can subdivision developers be required to do?
- *The city has all the possible tools in place to require parks to be provided in growing areas either through land dedication or assessment of SDCs. What we are missing is the plan to guide where those parks and facilities should be located and what they should include. The plan will help us use the fees we collect to meet our needs in a more logical and desirable way.*
- The City has made great strides in the last few years. These kinds of plans are important.

## **Public Meeting #1**

The public meeting is scheduled for March 1<sup>st</sup>. It will be a joint meeting with the planning process for downtown/highway 211 plan. Please help us get the word out! Encourage people to come to the Adult Center from 7-9 on Thursday, March 1<sup>st</sup>.

We will be producing meeting fliers, putting notice in the newspaper, and on the city's Web site.

Other means of communication were suggested, including

- The school building administrator,
- Chamber newsletter,
- Rotary/Kiwanis/Chamber lunches/announcements.

## **Next Steps**

We will next be preparing for the public meeting, doing some initial mapping, evaluating level of service standards and identifying priority needs.

**City of Molalla  
Parks, Recreation and Trails Master Plan**

**PROJECT ADVISORY COMMITTEE (PAC)**

**MEETING #2**

**March 12, 2007, 7-9 p.m.**

**In Attendance:**

*Committee Members*

Glen Boreth  
Steve Courtain  
Paul Eriksen  
Danna Jacober  
Steve Loutzenheser  
Dick Miller  
Maureen Phillips  
Jim Taylor  
Robert Trexler  
Randy Williams

*Staff and Consultants*

Mike Clarke, City of Mollala  
Ellie Fiore, Cogan Owens Cogan  
Matt Hastie, Cogan Owens Cogan  
Eldon Lampson, City of Molalla  
Shane Potter, City of Molalla

**Welcome and Project Update**

Matt opened the meeting by thanking everyone for coming and thanking those that attended the open house on March 1<sup>st</sup>. He briefly reviewed the agenda and added that he would like to establish priorities for trails during the meeting.

We do not yet have a summary of questionnaire responses since they went out later than anticipated. The response rate so far seems to be better than average. Questionnaires returned by mail aren't typically statistically reflective of the community because the responses aren't random, although a good response rate increases reliability. A high return rate lends more validity to the results.

Before our next meeting, we will send out a summary of the questionnaire responses with other materials.

## Public Meeting Summary

On March 1<sup>st</sup>, the city held a joint public meeting with the Downtown/OR 211 planning project team. We presented a brief overview of our project and had a station with maps for comments. Meeting attendees also participated in an exercise to express their priorities for different types of parks and facilities and for recreational programs.

Generally, meeting attendees expressed the most support for:

- Community center
- Sports fields
- Trails
- Community parks
- Health and wellness programs
- Senior programs
- Before- and after-school programs

Residents also expressed support for maintaining existing parks and facilities.

## Existing Conditions

We identified a variety of conditions for each park and/or recreation facility in the community, including city-owned and other facilities. They have been classified based on size, function and amenities.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- Does the questionnaire measure how strongly people feel about their response? How is this measured or analyzed?
- *Responses will be analyzed in the aggregate. Beyond priority scoring, it is difficult to assess the tone of a given response, although this can sometimes be captured in comment area through transcription. The results will be most useful in assessing collective priorities and support for specific types of facilities and programs.*
- People may not have understood the difference between a neighborhood and community park. This could have been better explained.

Matt explained that the purpose of documenting existing conditions is two-fold. First, it describes what the city has in terms of parks and amenities; second, it provides a baseline from which to estimate future needs. Mapping the existing facilities helps us consider the location of existing and future needed parks.

We have updated the park service area map based on feedback we received at the public meeting. Most park districts tend to have a larger radius for community parks, because these parks serve more people and a wider area. Community parks service areas don't assume that everyone can walk to these parks.

Currently, city-owned parks are concentrated on the east side of town, while the west side has more schools and playing fields, which aren't equivalent to neighborhood parks. The west side is underserved whether or not school facilities are included in the inventory.

- How do the service areas affect recommendations, since they overlap?
- *The intent is to identify where there are gaps in service—where people live but are not within the designated service area of a park.*
- Why is the Big Meadows park not included in the inventory?
- *It is a pocket park, based on its size and amenities. It is privately owned and not necessarily available for general public use. Including it won't entirely alleviate the need for parks in that area.*
- Color coding the schools differently than city-owned properties will better show the underserved areas on the maps.
- Is 1/3 of a mile too far for people to be from neighborhood parks?
- *It depends. We need to evaluate the distance standards based on the resulting impacts, including cost. Many other cities or park districts use a standard of ½ mile but some use ¼ mile.*
- It may help to look at overlapping service districts by outlining the buffers with a dotted line. Make the problems more clear visually.
- The current problems will become greater with growth. Some new residents are beginning to ask for parks, and the city has new tools. The promotion of a recreationally-themed community by the chamber and TEAM needs to be backed up by a strong park and recreation system.
- Do you consider hindrances to travel?
- *Yes. There are barriers within the areas shown. Access can be impeded even within the designated service area. Major roads are an example of barriers within a service area.*

- Please add sidewalk systems shown as trail connectors.
- Not too many people expressed interest in skate spots at the public meeting, but places for kids are important.
- Recreation is one of the most aggressive elements of the new comprehensive plan, so let's include a ¼ mile service area for neighborhood parks as an option.
- Can we state whether trails will be ADA accessible?
- Yes.
- Developing trails and facilities in stages would be good, for example, starting with soft surface trails that can be upgraded. Allow partial development so trails aren't prohibitively expensive.

### **Level of Service Standards and Needs Analysis**

Matt reviewed the information provided to the PAC. He noted that the National Park and Recreation Association (NRPA) guidelines are considered flexible guides rather than strict standards. Two scenarios were considered—one without any use of school facilities and one assuming 50% community use of school facilities. Existing level of services don't reflect condition of parks or facilities, only acreage. The use of playing fields will affect overall need. We need to verify the schools facility numbers to confirm what we heard from the PAC, i.e., that the city is short on playing fields.

The city's new comprehensive plan sets a total of 12.5 acres/1,000 residents as an overall level of service. We have prepared a range of options for meeting this goal. Standards for neighborhood parks typically range from 2 to 5 acres depending on community preference and conditions, and impacts on overall need and cost. We didn't propose varied fields standards.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- Do the NRPA standards change often?
- *No. The NRPA is moving away from standards towards more guidelines. They do respond to research and trends (especially for sports) that are incorporated but there is not too much change in standards for broad categories of facilities (e.g., neighborhood and community parks).*
- Parks and facilities in the city serve a broader population, not just the city population.
- The large open spaces make it look like the shortage of parks is not that serious. Be careful with public perception.

Matt explained that the preliminary needs analysis shows the acres/numbers of parks and facilities needed to meet the standard. Low and high figures for 2007 and for 2027 are presented.

By counting schools facilities, the city appears to have met its overall standards today, but with some gaps for particular types of parks. Without schools, it is pretty close to meeting the 12.5 acre/1,000 standard, but still short in certain areas.

To meet the overall standards, we varied the acres of “other types of parks” which includes the aquatic center, playing fields, etc. In order to meet the aggressive overall standards (12.5), the city should aim high for neighborhood and community parks. More community parks and playing fields are needed in any scenario.

- Funds will be generated by growth levy.
- The city can't rely on the public schools for use of its parks and facilities. They aren't under the jurisdiction of city council.
- *These issues can be addressed in the implementation/strategic plan. If you want to rely on schools, you will need to develop a formal agreement with them and partner in funding development and maintenance of the facilities that are available for some community use. An IGA can provide some certainty for both parties.*
- The school district serves a broad service area.
- The existing availability of school district properties is very limited, and will only get worse as the district grows. The school district may also require maintenance fees.
- School administration changes regularly and is a political body. Can change quickly.
- *Typically, both parties are required to overturn an IGA.*

Matt recommended presenting both alternatives to the public along with the associated costs. Two of the major cost components—acquisition and development—can be the responsibility of the development community. The costs of maintenance and improvement are a major challenge, and this is where most park and recreation districts or departments run into problems. The group agreed to keep the use of schools properties as an alternative to bring to the public and for further discussion with the school district, and to present the costs and challenges associated with each scenario. We will ask the school district for a more realistic number related to the percentage of time facilities would be available for general

community use. We also will ask them to consider how that may change in the future, and list the pros and cons for each scenario.

- Be more specific about the use of natural space and open space. Ivor Davies Park skews the figures a lot by making it look like there is no need for additional open space, but there is a need for open space buffers throughout the city.
- A major problem is that the overall needs are higher than the school district facilities can offer in combination with the city. We don't want to rely on them if they don't have the capacity (in terms of available space or time) for more general community use. The plan should reflect what the city needs. Although there has been a good historical alliance between the schools and the city, we may not be able to maintain that if the district sees mounting pressure for district use of its facilities.

Matt asked the group whether they wanted the city to be at the low end, middle or high end of the overall level-of-service standard. The city would like to keep the overall standard of 12.5 acres/1,000 residents.

- Maintenance costs are currently about \$60k per year, but performed fairly cooperatively. The parks department has no line item for maintenance; it is run out of the public works department. Fox Park is funded by a private foundation.
- The city already cooperates with other entities for some maintenance. At Sheets Field, for example, the high school uses the field for JV softball in exchange for some field care. It also is used by Molalla Youth Sports (MYS).
- Natural areas and open space will be gained automatically through dedications in subdivisions of un-developable wetlands, etc.

Matt asked the group whether neighborhood parks or community parks should be more highly prioritized in refining recommended level of service standards.

- The group expressed general support for more neighborhood parks and fewer community parks.
- A bigger problem than overall acreage is the location of parks. There will always be a deficit in the northwest area of the city where development has already occurred. Specify areas where parks are needed.
- It was agreed to keep an average standard for open space.

- Specify need for small open spaces as buffers between developed areas.

We will refine these standards and look at what falls into the other category and also look at costs and maintenance costs.

- The city needs need more baseball and softball fields to realize its vision of being a recreational center. It may be necessary to double the NRPA standards for these fields, and adding more multi-purpose fields for soccer and football. The basketball standards seem fine, but the standard for tennis courts also could be doubled.
- The existing tennis courts are in terrible condition.
- Tennis and the aquatic center are important recreation amenities, particularly for all ages and individualized recreational activities.
- How does the PAC communicate to the community so they can make informed decisions? What is recommended to get info out to as many people as possible?
- *We will work with the newspaper to get information on his project published, and use the water bill and city website to distribute information. The public meeting is a key opportunity to get public input, and we could potentially do a second survey if resources allow.*
- Tying this process to the comprehensive plan update process may also garner support, since there was a lot of support for the recreational community concept in that process.

## **Costs**

Matt presented rough estimates for the cost of some parks and trails.

- Acquiring and developing an acre of park is about \$300,000.
- A multi-use trail costs about \$100,000 per mile to develop.

We will refine these figures and provide information about alternatives and their costs as well as long-term maintenance implications. SDC's and state grants are funding options for development of new facilities and for some types of improvements. State funds don't need to be used for the existing population. With a \$140,000 grant from the state, we can leverage 140K of SDCs. These combined funds can be used to accommodate both growth and existing development.

## **Trails Priorities**

Each PAC and staff member stated their two highest trails priorities. The railroad corridor and Bear Creek received the most support. There was

also a good deal of support for creating a loop of trail and/or safe pedestrian routes and sidewalks around the city.

### **Next Steps**

We will refine and provide more detail on the scenarios we discussed today. We will expand on the trails plan and update the maps. For our next meeting, we will prepare more cost information and start on strategic/action plan elements that will go into the draft master plan. The group agreed that another PAC meeting and a public meeting should be held should be held to review alternatives (before the draft master plan is produced) and that the PAC would meet a fourth time (after the public meeting) if resources allow.

**City of Molalla  
Parks, Recreation and Trails Master Plan**

**PROJECT ADVISORY COMMITTEE (PAC)**

**MEETING #3**

**April 23, 2007, 7-9 p.m.**

**In Attendance:**

*Committee Members:*

Steve Courtain  
Wayne Dettwyler  
Paul Ericksen  
Wayne Kostur  
Richard Trexler  
Randy Williams

*Staff and Consultants:*

Ellie Fiore  
Matt Hastie  
Eldon Lampson  
Shane Potter  
Jessica Roberts

**Welcome and Project Update**

Matt opened the meeting by thanking everyone for attending and introducing Jessica Roberts from Alta Planning. After reviewing the agenda, Matt updated the group on project progress.

Since the group last met, COC and Alta have updated and refined the two alternatives, analyzed the results of the community questionnaire, refined preliminary cost analyses, and created some draft strategies to meet future park, recreation and trail needs.

**Draft Trails Plan**

Jessica explained that following the public meeting, Alta Planning identified opportunities and constraints in the City of Molalla that may affect the ability to create or improve potential future trail segments.

She discussed some highlights of this analysis. Schools and parks are typically important destinations for trails. In Molalla, Long Park, the Adult Center and the skatepark are great opportunities to connect to the trail system. The Downtown/Oregon 211 project will call for some facility upgrades, including upgrades to the pedestrian network that will also provide good connections to the future trail system. Potential trail destinations outside Molalla may include the Hardy Creek mountain biking trail area and Molalla River State Park. The property owner developing the Timber Town community is interested in having trails on that site as well.

Jessica noted that there are wetlands, especially around Bear Creek, which could present challenges for the development of trails in this area. Other constraints are trail crossings of major streets, such as at Main Street. These are common challenges for communities. Land availability is also a challenge for Molalla, especially in the northwest where land has been built out or wetlands prevented construction in those areas.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- How is the trails plan related to city boundaries?
- *Most of the proposed trails are within the City and/or UGB. Some destinations are shown outside the UGB.*
- *The city is looking at a potential UGB expansion, which could present further opportunities for trail development. The City also could partner with the County or others to help construct trail extensions outside the city.*
- What is the status of the industrial land south of Main Street on the rail corridor?
- *Part of downtown/Timbertown master planning process will reroute the trail, which will go where Shaver is today. There is a large truck repair facility in that location now, but the city is in discussions about relocating that use.*
- *There is also a rib shack off the highway on the rail alignment. It is a permitted use, and there is still 25 feet of right of way available.*

Next, Jessica reviewed the trails priorities map, which builds off public input and the opportunities and constraints analysis. The first tier priority trails are mostly connected to parks and schools and create a basic functional trail system in the town. Bear Creek Road also is a great opportunity.

Second-tier priority trails sections connect to and enhance the first group. Community connections to the northwest and along 211 are listed as a secondary opportunity but depend on ODOT funding and implementation. Comments from CAC members included the following. Responses from consultants are shown in *italics*.

- Where are people likely to connect to the trail system?
- We may want to show high-priority routes and intersections, such as 5th, Toliver & Shirley. A crossover planned for Shirley will be fully ADA accessible.
- Shirley also goes out to 211 past the Buckaroo grounds on the east side of town. Between Boulander Field and the Buckaroo grounds, there is an off-street asphalt path.
- Trails should be shown along property lines instead of across them.
- Security and safety issue could be a concern going along school grounds.
- There will be an upgraded facility near the Middle School.
- Jessica asked about linking to Molalla Forest Road.
- There is a project to run a lighted path from the north to south parking lots in Clark Park in the City's SDC plan, which also would connect the high school to Shirley.
- Why is the link between Buckaroo Stadium and Vaughn a medium priority?
- *We heard at the initial project meeting and public meeting that there is the desire to make trail loops. That trail section would help complete a trail loop.*
- What about an opportunity to link the high school, land lab and Buckaroo Stadium?

## Questionnaire Results

Over 200 surveys have been returned to the city and analyzed. This is a great turnout rate for a community the size of Molalla.

The questionnaire asked people about their priorities in a couple different ways. Overall, community preferences for facilities are quite similar, as indicated by very close scores on the questionnaire. Hiking, biking and walking trails and neighborhood and community parks were rated highest. The close scoring does not provide a strong basis for setting priorities for different types of facilities, but does indicate that the community prefers a balanced system including a variety of facilities.

There were clearer preferences expressed for recreational programs. Programs for subsets of the population (e.g., skateparks) typically rank lower. Before- and after-school programs and senior programs rated highest on the questionnaire. Health and wellness programs were ranked highly by open house participants.

Question #2 also asked about priorities. Scores were very similar to the results of the first question.

A majority of respondents (61%) favored maintenance and improvements of existing facilities relative to expanding the system through acquisition and development.

The questionnaire also asked residents to rank trail priorities and suggest destinations. These responses were similar to those from other sources. Molalla Forest Road and the railroad corridor were the most popular trail segments.

Clark Park was the city facility most frequently cited as needing improvements.

### **Preliminary Draft Report**

The Preliminary Draft Report includes updates and narrative of what we talked about at the last advisory committee meeting, including a description of existing conditions.

The report also includes an updated set of proposed level of standards. At the last meeting, the group discussed a range of standards. The updated document includes proposed standards based on input from the public and the PAC. These include slightly higher standards for open space and for neighborhood parks. This will enable the city to meet its goal of providing neighborhood parks near all residences and preserving open space throughout the community. COC also developed proximity standards for neighborhood and community parks, which reflect community preferences.

Proposed playing field standards are somewhat higher than those proposed by the National Recreation and Parks Association (NRPA). This is in response to discussion at the last meeting about developing a recreation theme for the city. The proposed standards are 20 to 35% higher than NRPA guidelines, based on the type of field. Comments from

CAC members included the following. Responses from consultants are shown in *italics*.

- Baseball/softball and football/rugby/lacrosse standards may still not be high enough. What about moving to standards of 1/1,000 residents for baseball/softball and 1/3,000 for football/rugby/lacrosse.
- *These would be very high standards.*

The draft report includes two alternatives. Alternative A assumes the city meets its projected needs independently (i.e., it does not assume that city residents will have regular access to school district properties). This affects the need for playing fields and land associated with these and other recreational facilities that the school district could help provide.

Alternative B assumes that school district facilities would be used by the community approximately 30% of the time and there would be some form of financial partnership between the school district and city to develop, operate and maintain schools with joint use. Generally, school district-owned facilities don't meet the same recreational needs as neighborhood and community parks, although it could be possible to improve or expand some school district facilities to help serve this function.

In both cases, we assume that Ivor Davies continues to meet a significant portion of the city's open space needs and that other open spaces will be dedicated throughout the city as development occurs. Comments from CAC members included the following. Responses from consultants are shown in *italics*.

- The figures for tennis/basketball courts are flipped in the tables.
- Clark Park serves some neighborhood park functions; it could be on the neighborhood services map.
- It should be noted in the report that some of this land need will be met with land that is currently outside the existing city limits/UGB, assuming the UGB will be expanded over the next 20 years to accommodate the growing population.
- *Additional land also will be needed to maintain the proximity standard as Molalla grows*

### **Cost Estimates**

The costs in the preliminary draft report are very rough estimates. Costs to develop and maintain facilities can vary significantly for different types of

facilities and in different locations. The figures in the draft report are based on estimates from other park districts or departments. Development and maintenance costs will vary depending on what amenities the city provides. Land acquisition costs are more uniform and are based on local estimates of average costs per acre of land.

Overall costs are somewhat lower under Alternative B because in this scenario, some existing school district land and facilities help meet overall needs. This alternative also assumes that the city spends some money for operations and maintenance on school district properties, but benefits from some economies of scale due to shared use, resulting in a lower per acre maintenance cost (about 90% of the cost per acre assumed in Alternative A). Comments from CAC members included the following. Responses from consultants are shown in *italics*.

- It should be noted that city code requires subdivision developers to donate or provide their share of park land and improvements. Some capital costs will be assumed by development.
- *One of the next steps will be to look at how much of the projected costs will be absorbed by dedication of land, fees in lieu of dedication and SDC's. State law requires that SDC's be based on continuing to provide the City's current level of service, rather than a future proposed level of service. Other funding sources like grants and community partnerships also are possible funding source.*
- How are operations and maintenance costs estimated?
- *Generally, they are estimated on a per field per year basis, assuming average costs provided by other communities or park and recreation districts. We multiplied these per year costs by the number of field called for. We also assumed that fields would be grass fields, rather than synthetic fields. Building artificial turf fields saves money on life cycle and maintenance costs and provides more overall capacity because those fields don't need to be rested in the same way that grass fields do. However, they are much more expensive to build and replace than grass fields.*
- *Multi-use fields can help reduce development and maintenance costs, but create other challenges such as not enough rest and user conflicts. Our calculations of need assume the use of dedicated fields. The number of needed fields could be reduced if we assume that fields would be used for multiple purposes during the year (e.g., soccer in one season and softball in another).*
- There was some confusion around the cost estimates, particularly the difference between the two.
- Partnering with the school district is a potential way to reduce costs.

## Public Open House Preparations

We will present an updated plan and other materials to the public at the meeting on May 14<sup>th</sup>. We will solicit feedback on the two alternatives and answer any questions from meeting attendees.

The group agreed that for the format for the May 14<sup>th</sup> public meeting will be a brief presentation followed by a question/answer period and an open house.

- The first two questions on the questionnaires would be good to ask at the public meeting.
- We may want to explain that the school district and city have different service areas and this why there is somewhat of an issue with sharing facilities.
- We should have a good handle on alternative cost figures by the next meeting, including field turf vs. grass.
- Molalla Youth Sports has their fields lined in partnership with the school district—they buy the paint and the school district provides the labor.
- *This type of creative partnership is needed to continue to provide facilities in a cost-effective way.*
- There are presently disputes over the use and scheduling of softball, baseball and soccer games. Many of the city and school district fields are essentially multi-use fields now.
- Specific cost information may help community members choose a preferred alternative. Molalla is a fiscally conservative community.
- *Matt and Shane will discuss city financing. This plan will help make the case for funding of facilities at the levels of service recommended by the community.*

## Draft Strategic Planning Element

The strategic planning element includes plan goals and policies to meet these goals. It addresses all aspects of the recreational community. The goals address meeting the standards we've discussed and guide the process of identifying amenities for those parks. One of the goals is to explore developing a community center, based on public input. Another policy recommendation is to regularly check in with the public on recreational programming needs and preferences.

- What about implementing a two-tiered user fee based on city residency?

- *This is an option, though this shouldn't be applied to school district facilities. Some facilities don't lend themselves to user fees. Fees and the percentage of costs captured by fees will vary by type of facility. Some facilities may have no fees. Fees for other facilities may capture a portion of the cost to operate them. In other cases (e.g., wedding reception or other event facilities), fees may exceed operating costs and be used to subsidize other facility costs. We can provide more guidance on fees in subsequent drafts of the Park and Recreation Plan.*
- What about SDCs for commercial development?
- *We'll need to check scope of commercial SDCs.*

This document is the starting point for goals and strategies.

### **Next Steps**

We will refine information based on PAC input and gather more cost information in preparation for the open house/public meeting.

At our next public meeting, we will review the draft plan with the committee. The next (and last) CAC meeting is scheduled for Monday, July 9 at 7:00 p.m.

## Molalla Parks and Recreation Master Plan PAC Meeting #4

### **In Attendance:**

#### *Committee Members:*

Robert Trexler  
Steve Clark  
Paul Ericksen  
Wayne Kostur  
Danna Jacober  
Glen Boreth  
Dick Miller

#### *Staff and Consultants:*

Matt Hastie  
Ellie Fiore  
Shane Potter  
Mary Robertson  
John Atkins

### **Welcome and Project Update**

Matt welcomed everyone, thanked them for attending the final PAC meeting and provided a project update. A second open house was held in May to gather information on the two plan alternatives. A summary of that meeting is available and was provided to PAC members separately. Since the last PAC meeting, we have expanded on the preliminary draft plan, adding information on approach, costs and funding strategies. There are some placeholders in the draft plan right now where we need to develop additional information related to costs. Some maps also need to be added.

### **Public Meeting Summary**

Ellie discussed the feedback the team received at the second public meeting. There was general support for the overall approach and for the number of neighborhood and community parks recommended. There was slightly more support for Alternative A, in which the city meets its park and recreation needs independent of the school district.

Meeting attendees supported developing more baseball/softball fields than were projected in the draft alternatives. They tended to feel that the estimated numbers of soccer, and football/rugby/lacrosse fields were about right, and that fewer tennis/basketball courts were needed.

There was support for each of the trail priorities except the rail corridor to high school connector, high school to land lab connector, and the Buckaroo stadium to S. Vaughn Road link. Meeting participants agreed with most priorities, but there was some disagreement about equestrian use on trails and maintenance agreements with community groups.

## **Draft Plan**

Next, Matt reviewed key elements of the draft plan. This draft includes a lot of information on trails, as they are a major component of the proposed system. The level of service standards we discussed as a group are included as well as playing field standards that are higher than the national standards.

Two approaches to meeting the established needs were evaluated—with and without school district facilities. Although alternative A (independent of the school district) was preferred, we recommend exploring partnerships with the school district, particularly in targeted parts of town (e.g., west of Molalla Avenue) to meet neighborhood park needs. There also may be some efficiency to be gained from continued joint operation and maintenance and/or other partnerships associated with selected facilities.

The idea of a community center was supported in public meetings and among PAC members. Given the population of Molalla, a community center would be more modest than others in the region. The draft plan contains guidance for the city to evaluate the feasibility of a community center and design it to meet a variety of community needs. We also recommend considering locating the community center on the aquatic center property.

More trails material standards and guidelines will be included as appendices to the plan. The report will also include an appendix with implementation strategies specific to the rail corridor trail.

## **Cost and Funding Sources**

Matt explained that we have modified some cost figures assuming some mixed-use fields and based on recent capital improvement plans prepared by the city. Maintenance figures for tennis courts are missing from this draft.

We also estimated what percentage of future funding would come from various sources. The table on page 37 had a misprint. Land dedication should account for 30% of all overall costs. The system development charge figures assume substantial increases in SDCs. We recommend that they city revisit how SDCs are calculated, given the costs associated with this plan.

Donations and grant estimates are based on recent fundraising trends. Operations and maintenance costs are more difficult to estimate. The city needs to determine how much it can provide from general funds and other sources. It

should consider user fees and other funding options. One option may be to establish a separate parks and recreation district, or have the North Clackamas Park and Recreation District annex the Molalla parks area.

Committee members noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- Chapter 6 should be referenced in the executive summary.
- Make the plan easier to read in black and white.
- The city code requires land dedication and improvements. This could result in the need for SDC credits. Would it be more logical to increase the proportion of land dedication and reduce the share of SDCs contributing to the overall funds? The draft plan currently supports SDC increases.
- *SDCs need to be based on existing level of service and on planned capital improvements. Right now, Molalla's rate is lower than what it could be. There is also a "cap" on what residents are willing to pay. Some cities and districts don't charge the full rate because they have judged that higher fees will not be tolerated. All of these issues will need to be considered in updating the City's SDC rates and determining the most appropriate mix of funding sources, including land dedication requirements.*
- IGAs can be very difficult to develop and keep functional. The city has worked on IGAs between jurisdictions and has had mixed experience.
- *It is generally best to agree on the terms of the agreement and rely on legal input later in the process. We suggested considering an IGA particularly for Alternative B, but the city may still want to formalize relationships and agree on terms for the preferred alternative.*
- The city and school district have both formal and informal agreements. There are formal agreements regarding the aquatic center, but a looser agreement for Fox Park where the city provides water for the grounds and the school mows the grass. So far there has been a good partnership between city and school district.
- *We can add some material on the character of MOUs and IGAs and the process for establishing them.*
- The water bill questionnaire didn't reach non-residents. A lot of people outside the city limits do business here or use the aquatic center and other community resources. The school district also draws from wider area. Danna distributed the questionnaire through Molalla Youth Sports and some public meeting attendees also came from outside the city. The city potentially could get fliers widely distributed through Thriftway, Safeway, the senior center and the library.
- These are good ideas to announce the planning commission and city council hearings. We will also get the draft document on the city's website and post the link on other materials.
- This is a living document. The plan is a guide that the city should revisit and update periodically. Although the planning period is 20 years, the city shouldn't wait that long to update the plan.

- *We will reiterate this in the plan.*
- Given the uncertainties with the pool and potential annexation, is it worthwhile to prioritize the elements of the plan?
- *We can recommend some priorities while preserving flexibility. We will look at the priorities from the public input.*
- This is a good, thorough comprehensive plan.

## **Schedule**

Shane estimated the planning commission meeting would be held at the end of August or mid-September based on noticing requirements. Since there has been good participation by city council and planning commission members, more than 1-2 meetings probably will not be needed. It should be about a two month process.

## **Next Steps**

We will make changes to the plan based on this discussion with the PAC, add missing information, include information on priorities and work with the city to provide notice to city residents and other interested parties using a variety of methods.

## Appendix B

# City of Molalla Parks, Recreation and Trails Master Plan

## SUMMARY OF PUBLIC MEETING #1

On March 1<sup>st</sup>, city staff and consultants held a joint public meeting with the Downtown Molalla/OR 211 planning team. Matt Hastie of Cogan Owens Cogan gave a brief presentation that provided an overview of the planning process for the park, recreation and trails master plan. He discussed the project's objectives, schedule, main elements and preliminary findings. Approximately 45 people attended the meeting.

Meeting attendees provided the planning team with feedback on park locations and amenities and reviewed the initial trails concepts. Participants suggested additional trail/pedestrian routes in the following locations:

- Along Main Street/Hwy 211 west of downtown to Hwy 213
- Along Hwy 213 from Main to Vick St
- On Vick Street between Hwy 213 and Molalla Ave
- On 5<sup>th</sup> Avenue between Lowe and Mathias
- Along Shirley Street to Bohlander Field and north to Vaughn
- Between Hwys 211 and 213 southwest of the elementary school (along wooded corridor).

A map of these locations will be provided to project advisory committee members at their second meeting.

Participants also were asked to identify priorities for parks and facilities and recreational programming. The following were identified as high priority needs:

- Community center
- Sports fields
- Trails
- Community parks

- Health and wellness programs
- Senior programs
- Before- and after-school programs

Residents also expressed support for maintaining existing parks and facilities.

# City of Molalla Park, Recreation and Trails Master Plan

**PUBLIC MEETING #2**  
May 14, 2007, 7-9 p.m.

## **MEETING SUMMARY**

### **Welcome**

Shane opened the meeting by thanking the attendees for coming and explained that this is the second of two public meetings for the Park, Recreation and Trails Master Plan. The goal of the meeting was to get feedback on the two alternatives in the draft master plan and to express other community preferences related to parks, recreation and trail facilities and issues.

### **Presentation**

Next, Matt Hastie presented information on the parks and recreation master plan including project objectives, timeline and existing conditions for parks and recreation in Molalla. He also described the proposed level of service standards for the city and compared them to national guidelines. He then presented the two alternative plans and their associated facility needs and estimated costs and explained that we would like feedback on the alternatives.

Jessica Roberts presented trail priorities that were developed with public input and highlighted key opportunities and constraints for the proposed trail system.

Matt also highlighted some potential funding tools and strategies and presented the draft plan goals that we would like comments on.

### **Question and Answer Period**

Meeting attendees noted the following comments and questions (responses from staff and consultants are shown in *italics*).

- What input did you get from the public on recreation?
- *Senior programs and before- and after-school programs for youth received the most overall support. Health and wellness programs were highly ranked at the public meeting.*

- The off-road trails will be too narrow for vehicles. What does this mean for policing? Are the trails going to create opportunities for crime?
- *This is a common concern in other communities in which we have worked. There are a lot of strategies for ensuring trail security, including lighting and community partnerships for monitoring the trails. Alta Planning will prepare a security memo as part of their work on this project. Many towns find that active use of trails actually helps curtail unwanted activities through more active use.*
- Safety also will be an issue for homes located next to trails.
- Do the system development charges (SDCs) listed include schools?
- *Schools cannot assess SDCs under current law. SDCs are used to pay for facilities necessitated by new growth, including improvements to existing facilities, new facilities and planning costs.*
- It seems like a lot of the financing tools are dependent on development. If our plan is aggressive, will this discourage development here due to higher associated costs? Also, are people likely to vote for funding for parks when they have not voted for school funding?
- *Most communities in Oregon use SDCs, which are based on the current level of service. It is difficult to predict what people will vote for. Generally, public safety, schools and park and recreation garner the most public support.*
- *Another funding strategy used by some communities are separate park and recreation districts which levy their own taxes.*

## Open House

Meeting attendees were encouraged to visit different stations, ask questions of staff and consultants and to record their preferences on the comment forms and posters.

The results of these exercises are presented below.

- *Parks and Open Space:* Alternative A received 2 votes, while Alternative B received 1.
- *Playing Fields:* Alternative A received 4 votes; Alternative B did not receive any votes.
- *Playing Fields:* Meeting attendees tended to support developing more baseball/softball fields than were projected in the draft alternatives. They tended to feel that the estimated numbers of soccer, and football/rugby/lacrosse fields were about right, and

that the tennis courts/basketball court needs were overestimated.

- *Trail Priorities:* There was support for each of the trail priorities except the rail corridor to high school connector, high school to land lab connector, and the Buckaroo stadium to S. Vaughn Road link.
- *Strategies and Policies:* Public meeting participants supported most of the draft goals and policies. Items on which there was disagreement included the following:
  - Work with local community groups and neighboring residents to develop partnerships for maintaining neighborhood parks.
  - Designate selected trails for equestrian use.
  - Increase the number of sports fields of all types throughout the city, consistent with level-of-service standards identified in this plan. (*Note: This was somewhat contradictory to the result of the sport fields question above.*)

## Compilation of Public Meeting Results

Parks and Open Space: Which alternative do you prefer?

<b>Alternative A: City-Owned Facilities Only, Future Land and Facility Needs</b>	<b>Alternative B: Community Use of School District Properties/Facilities</b>
2	1

Playing Fields: Which alternative do you prefer?

<b>Alternative A: City-Owned Facilities Only, Future Land and Facility Needs</b>	<b>Alternative B: Community Use of School District Properties/Facilities</b>
4	

Do you think estimated fields needs are about right, more than necessary or less than necessary?

<b>Field Type</b>	<b>About Right</b>	<b>More than necessary</b>	<b>Less than necessary</b>
Baseball/Softball Fields	1		3
Soccer Fields	2		1
Football/Rugby/Lacrosse Fields	2		1
Tennis Courts	1	1	2
Basketball Courts	1	1	2

Trails: Do you agree or disagree with each of the trail priorities listed in the table below?

<b>Highest Priority: These trail segments represent the best opportunities to develop trails and to create a functional city-wide trail/pedestrian system</b>		
<b>Trail Segments</b>	<b>Agree</b>	<b>Disagree</b>
North-south rail corridor	4	
Bear Creek corridor	4	
Cole Avenue	1	
Rail Corridor to High School connector		1
High School to Land Lab connector		1
<b>Medium Priority: Medium priority trails will add important connections, safe pedestrian routes and link the trail network to popular destinations</b>		
S. Vick Road	3	
Highway 213 between Vick and Highway 211	4	
Molalla Forest Road	3	
Buckaroo Grounds to S. Vaughn Road		2
<b>Community Connectors: These are on-street connections with sidewalks and bike lanes that link to trails network</b>		
Shirley Street	1	
Toliver Road	1	
Leroy Avenue	1	
Ridings Avenue	1	
E. 5th Street	1	

Draft Strategies: Do you agree or disagree with each of the trail priorities listed in the table below?

<b>Goal: Provide neighborhood and community parks to serve all residents of Molalla.</b>		
<b>Priorities</b>	<b>Agree</b>	<b>Disagree</b>
Provide neighborhood parks at a standard of 3 acres/1000 and within ¼ to ½ mile of all city households.	5	2
Provide community parks at a standard of 3 acres/1,000 residents and within 1 to 1 ½ mile of all city households.	4	1
In planning for new parks, consider the impact of arterial roads and state highways as barriers to nearby parks and locate parks to minimize and or take into account such barriers.	5	
Provide other parks and facilities (such as community center, skatespots and others) consistent with this plan.	3	1
Work with neighboring residents to design parks and park amenities to meet local community needs.	5	
Regularly maintain neighborhood and community parks and amenities within them.	3	1
Work with local community groups and neighboring residents to develop partnerships for maintaining neighborhood parks.	3	2
Make targeted improvements to existing parks and recreational facilities consistent with community priorities.	4	1

**Goal: Develop and maintain a city-wide system of trails to provide recreational opportunities such as walking, bicycling, jogging and horseback riding and to encourage safe alternative modes of transportation, consistent with community priorities identified in this plan.**

<b>Priorities</b>	<b>Agree</b>	<b>Disagree</b>
Attempt to make some trails fully accessible to people of all abilities.	4	1
Link trails to community destinations such as parks, schools, and shopping areas.	4	
Furnish trails with amenities such as interpretive and directional signage, benches, parking and other services.	4	
Link trails to on-road bicycle lanes and sidewalks/pedestrian routes; use on-street facilities to provide links between off-street trails.	2	
Designate selected trails for equestrian use.	3	2
Establish easements for trails in developing areas as part of the development approval process, consistent with proposed trail corridors identified in this plan.	3	

**Goal: Provide sports and recreation facilities and programs for city residents of all ages and abilities.**

<b>Priorities</b>	<b>Agree</b>	<b>Disagree</b>
Offer recreational programs and activities to meet a range of community needs, including continuing to offer programs for seniors at the Adult Center and creating before- and after-school programs for city youth.	7	
Regularly assess the programming needs and priorities of residents in establishing and refining program offerings.	1	
Improve ADA accessibility for city parks and facilities.	4	1
Explore the possibility of developing a community center.	5	
Increase the number of sports fields of all types throughout the city, consistent with level-of-service standards identified in this plan.	2	2
Explore partnerships with the school district, Molalla Buckeroo or other entities for joint development, use, maintenance and operation of facilities; formalize such partnerships in intergovernmental agreements (IGAs).	5	
Ensure the safety of park users and provide adequate policing of parks and trails; explore opportunities to co-locate park or trail facilities near public safety offices (e.g., police or emergency service offices).	3	

<b>Goal: <i>Preserve open space throughout the city</i></b>		
<b>Priorities</b>	<b>Agree</b>	<b>Disagree</b>
Continue to maintain and improve Ivor Davies Park as the city's primary community open space facility. Explore opportunities for expanding the park and/or facilities within it to better utilize it as open space.	4	
Acquire open spaces to serve as buffers between developed areas.	3	1
Require dedication of open spaces/parks in housing developments, consistent with zoning and subdivision ordinance requirements.	3	1

<b>Goal: Provide adequate funds to develop, improve, maintain and operate park and recreation facilities, consistent with city goals, policies and standards.</b>		
<b>Priorities</b>	<b>Agree</b>	<b>Disagree</b>
Use a variety of funding sources and tools to acquire land for and develop park and recreational facilities, including system development charges (SDCs), grants, bond measures, private donations and other mechanisms.	3	
Establish adequate on-going funding for regular maintenance and operation of facilities as well as large-scale capital improvements. Options may include dedication of general funds, use of a special district, community partnerships and other means.	3	1
Establish user fees, where appropriate to recover costs for use of recreational facilities to the greatest extent reasonably possible.	4	
Prioritize maintenance and improvement of existing facilities over construction of new facilities.	4	

## Park, Recreation and Trails Master Plan

# Public Meeting: Park, Recreation and Trails Master Plan

Help decide the future of our  
park and recreation system!

Tell us about your priorities for trails,  
parks and recreation facilities.

**Thursday**

**March 1<sup>st</sup>, 2007**

**7 to 9 p.m. Molalla Adult Center  
315 Kennel Avenue, Molalla**

This meeting is also an opportunity to  
comment on preferred land use and  
transportation concepts for the downtown area  
and for OR 211 and designs for the Main Street  
and Molalla Ave. intersection.

Refreshments provided.  
For more information, please contact Shane Potter at the City of Molalla  
(503) 829-7526, or see the City's Web site <http://www.molalla.net/>. We  
hope to see you there!

# City of Molalla

## Park, Recreation and Trails Master Plan

### **Public Meeting: Park, Recreation and Trails Master Plan**

**Help decide the future of our  
park and recreation system!**

Give us your input on alternatives for the  
master plan and elements of the proposed  
trails system.

**Monday  
May 14<sup>th</sup>, 2007**

**7 to 9 p.m. Molalla Adult Center  
315 Kennel Avenue, Molalla**

Refreshments provided.  
For more information, please contact Shane Potter at the City of Molalla  
(503) 829-7526, or see the City's Web site <http://www.molalla.net/>.

**We hope to see you there!**

## Appendix D

# M E D I A   R E L E A S E

FOR IMMEDIATE RELEASE

### **Public Meeting on Molalla Park, Recreation and Trails Master Plan May 14, 2007**

### **Public Meeting on How to Meet Future Park, Recreation and Trail Needs and Priorities**

Molalla, OR – The City of Molalla is developing a Park, Recreation and Trails Master Plan. A **public meeting** will be held on **May 14<sup>th</sup> at 7 p.m.** with city staff, consultants and project advisory committee members to evaluate plan alternatives and proposed priorities for the trail system. The meeting will be held at the **Adult Center, 315 Kennel Street**, and consist of an informational **presentation at 7 p.m.**, followed by an open house with staffed displays on plan elements.

The Park, Recreation and Trails Master Plan process will include an overall vision for the recreational community as well as proposed facility standards, a long-term needs assessment and key opportunity sites for parks, recreation facilities, trails and open spaces. The plan will include policies and goals that are consistent with the City's overall goals and results of this and related planning efforts, and implementation strategies that help ensure the proposed park, recreation and trail system facilities can be developed and operated successfully. The trails planning element of the document will identify priority trail corridors and on-street connections between them, opportunities and constraints to implementation of trail projects, and a variety of tools to help pay for them.

All residents, park users and other community members with an interest in the future of the city park and recreation system are encouraged to participate.

Additional information can be found on the city's website at <http://www.molalla.net/~city/> or by contacting Shane Potter, City of Molalla, at 503-829-7526 or [planner@molalla.net](mailto:planner@molalla.net) or Ellie Fiore with Cogan Owens Cogan at 503-225-0192 or [ellie.fiore@coganowens.com](mailto:ellie.fiore@coganowens.com).

Appendix E

# City of Molalla

## Parks, Recreation and Trails Master Plan Questionnaire

The City of Molalla is currently undertaking a Park, Recreation and Trails master planning process. The goal is to craft a plan that truly reflects the needs and desires of Molalla residents and lays the groundwork for future development and operation of these important facilities and programs.

The city Planning Commission's vision is for Molalla to be a complete recreational community with neighborhood and community parks, recreation centers, and trails running throughout the city and to surrounding destinations.

Results of this questionnaire and other research, analysis and public involvement efforts will be used to prioritize future needed facilities, programs and improvements for the City of Molalla.

1. Using a scale of 1 to 5, where 1 means a very low priority for the city and 5 means a very high priority, please rate the relative importance of each of following types of facilities and programs. Please keep in mind that while all of these programs and services are important, funding is limited and must be allocated to meet a wide variety of needs. You may use each ranking more than once.

- |   |   |
|---|---|
| <input type="checkbox"/> Neighborhood parks   | <input type="checkbox"/> Sports fields  |
| <input type="checkbox"/> Swimming pools and aquatic programs  | <input type="checkbox"/> Skate parks  |
| <input type="checkbox"/> Community parks  | <input type="checkbox"/> Early childhood development classes for toddlers and parents |
| <input type="checkbox"/> Open space and natural areas   | <input type="checkbox"/> Before and after school programs for youth                   |
| <input type="checkbox"/> A community center that provide programs like youth sports, yoga, arts and crafts, dance, and general fitness programs | <input type="checkbox"/> Arts and crafts programs                                     |
| <input type="checkbox"/> Hiking, biking, and walking trails   | <input type="checkbox"/> Health and wellness programs                                 |
| <input type="checkbox"/> Equestrian trails  | <input type="checkbox"/> Nature and outdoor education programs                        |
| <input type="checkbox"/> Senior programs  | <input type="checkbox"/> Other  |

2. Please rank the following types of facilities from 1 to 7 in priority in terms of the need for each in the City of Molalla, where 1 means that there is the greatest need, and 7 means that there is the least relative need for that type of facility. Please use each ranking only once.

- |   |  |
|---|--|
| <input type="checkbox"/> Neighborhood parks   | <input type="checkbox"/> Swimming pools and aquatic programs                               |
| <input type="checkbox"/> Community parks  | <input type="checkbox"/> Trails, including pedestrian, equestrian and bike paths or trails |
| <input type="checkbox"/> A multi-generational community center that provides programs like youth sports, yoga, arts and crafts, dance, and general fitness programs | <input type="checkbox"/> Sports fields   |
| <input type="checkbox"/> A new or expanded senior center  |  |

3. Do you feel that improvements to existing parks and facilities or acquisition and development of new parks and facilities should be a higher priority? *(check the appropriate choice)*

Improvements to existing parks and facilities  
 Acquisition and development of new parks and facilities

4. The city is interested in establishing a strong system of trails that provide recreational opportunities for biking, hiking, jogging and equestrian uses and that links parks, recreation and other community facilities. The following roads and destinations have been identified as potential elements of the trail system. Please identify the relative priority for each one on a scale of 1 to 5, where 1 means a very low priority for the city and 5 means a very high priority:

Molalla Forest Road (potential destination/connecting trail)  
 Vaughn Road (potential trail)  
 Molalla Avenue (potential trail)  
 Former railroad tracks (potential trail)  
 Elk Farm (potential destination)

Please note any additional suggestions for important trail locations or destinations below.

5. Are there particular areas of the city that need additional park and recreation facilities, or existing parks and facilities that need improvement?

6. Please use the remaining space to make any additional comments about park, recreation and trail needs in the city that you would like staff to consider during the master planning process.

Please return your completed survey by mail or fax **by March 23<sup>rd</sup>** to:

Cogan Owens Cogan  
813 SW Alder Street, Suite 320  
Portland OR 97205  
Fax: 503-225-0224

## City of Molalla

# Parks, Recreation and Trails Master Plan

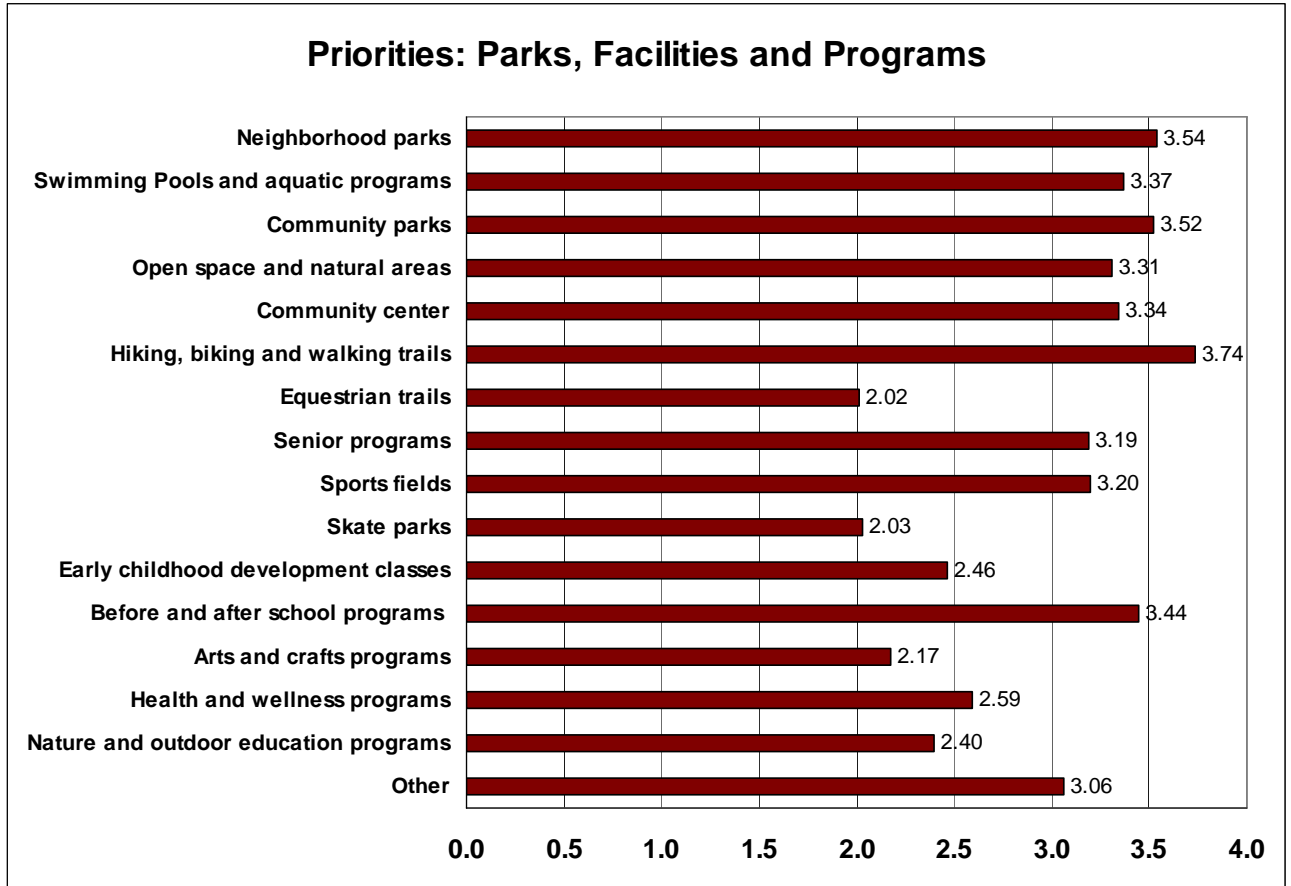
### Summary of Questionnaire Responses

This document presents an overview of the parks and recreation master plan questionnaire responses. A total of 206 completed questionnaires were received and summarized. Questions are listed below in **bold** type followed by graphic and/or narrative summary of the responses.

Key findings from this effort include the following:

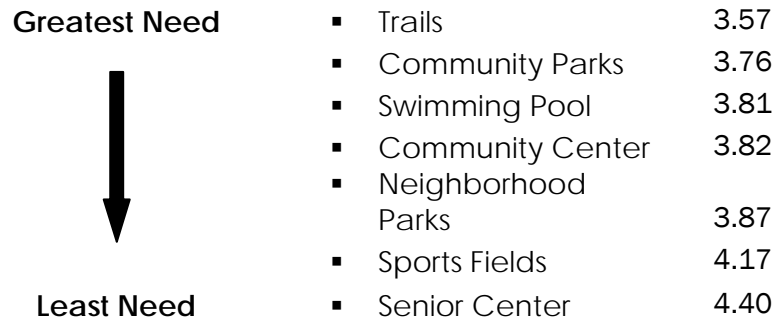
- Respondents indicate very similar levels of support for most types of recreational facilities, although hiking, biking and walking trails (average score of 3.74 out of 5), neighborhood parks (average score of 3.54) and community parks (average score of 3.52) rank highest, in that order.
- After-school and senior programs program rank highest in terms of different types of recreational programs with scores of 3.44 and 3.19 out of five, respectively.
- In prioritizing facilities, trails and community parks rate highest, while a new senior center ranks lowest in comparison to other facilities.
- Improvement of existing facilities is a higher priority for most respondents (61%), comparison to acquisition or development of new recreational facilities.
- The railroad corridor and Molalla Forest Road trail corridor are the top two trail priorities for respondents.
- In identifying other park and recreational issues and priorities, respondents most often said that additional parks and facilities are needed in or near Big Meadows, Lexington Estates and new housing developments in the northwest part of the city. Some respondents also noted the need for additional parks on the east end of the city. Clark Park was cited most often as an existing park in need of improvements.

- Using a scale of 1 to 5, where 1 means a very low priority for the city and 5 means a very high priority, please rate the relative importance of each of following types of facilities and programs.

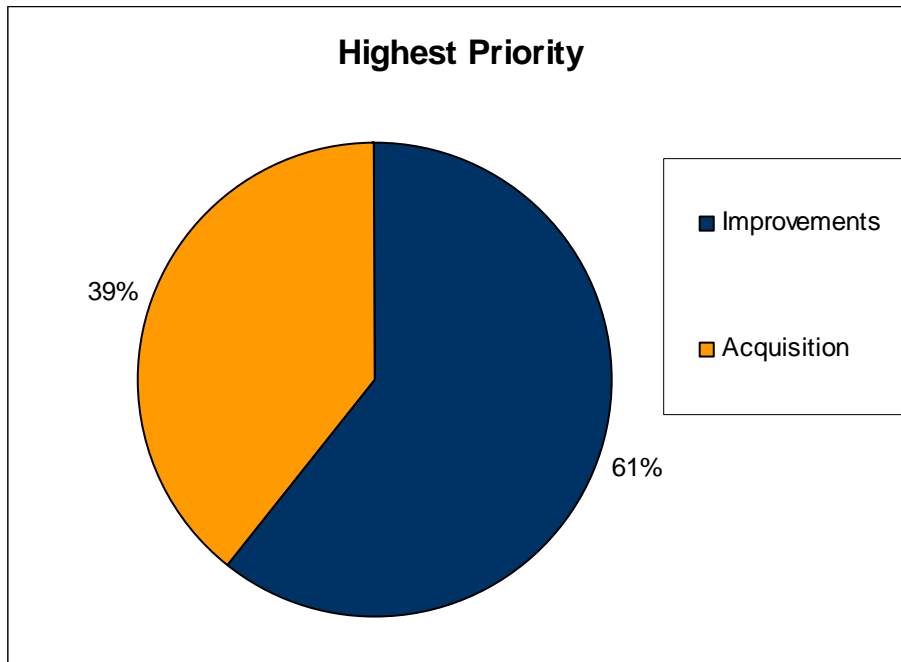


2. Please rank the following types of facilities from 1 to 7 in priority in terms of the need for each in the City of Molalla, where 1 means that there is the greatest need, and 7 means that there is the least relative need for that type of facility.

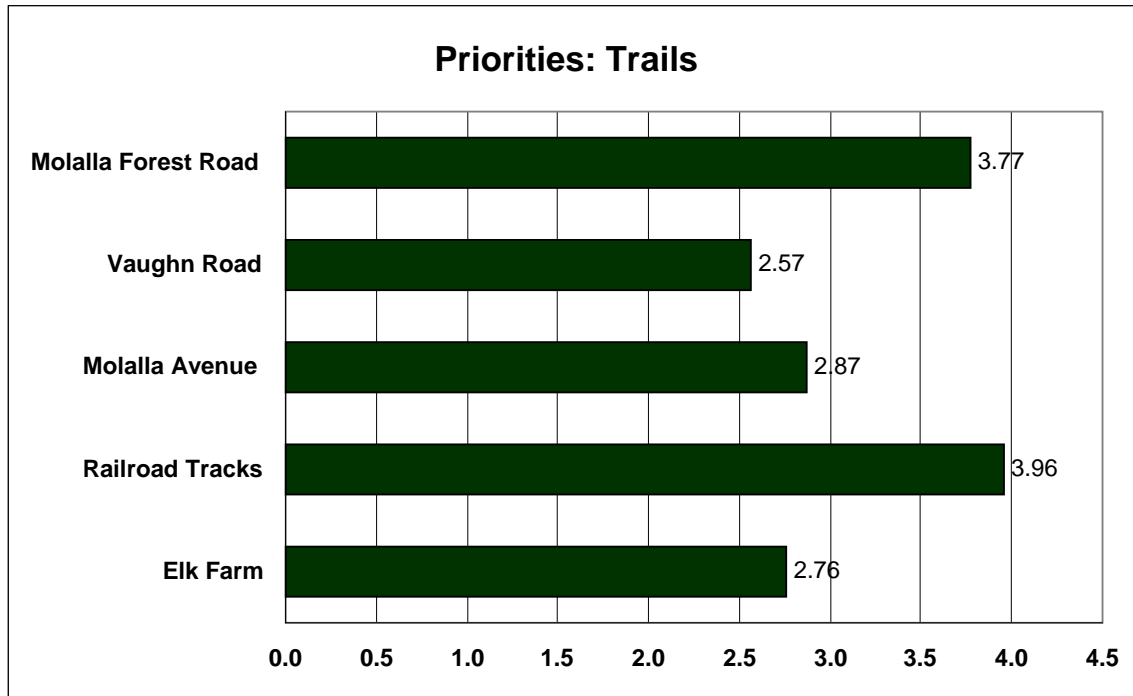
*Note: Several respondents expressed confusion with this item or scored the item incorrectly. The following graphic shows the relative scoring of the items according to the completed questionnaires.*



3. Do you feel that improvements to existing parks and facilities or acquisition and development of new parks and facilities should be a higher priority?



4. The city is interested in establishing a strong system of trails that provide recreational opportunities for biking, hiking, jogging and equestrian uses and that links parks, recreation and other community facilities. The following roads and destinations have been identified as potential elements of the trail system. Please identify the relative priority for each one on a scale of 1 to 5, where 1 means a very low priority for the city and 5 means a very high priority.



Please note any additional suggestions for important trail suggestions.

Proposed Trail Segments:

- S.Molalla Ave to Molalla Forest Road
- Toliver to 213 to Safeway to 211 to Leroy
- Toliver to Dryland
- Along Molalla Road, Bear Creek and Vick Road
- Big Meadow to Safeway
- Bear Creek
- Big Meadows to 213 and 211
- Vick Road
- Mathias Road to South Claim Road to Adams Cemetery Road
- 211 to 213
- Onaway

Proposed Destinations:

- Safeway Shopping Center
- Freyer Park
- Table Rock Wilderness trailhead
- Joyce Lake trailhead
- Peacock Lookout trailhead
- Aquatic Center
- American Wildlife Foundation
- Adams Cemetery Road

**5. Are there particular areas of the city that need additional park and recreation facilities, or existing parks and facilities that need improvement?**

The most common response to this question was that additional parks and facilities are needed in or near Big Meadows, Lexington Estates and new housing developments in the northwest part of the city. Some respondents also noted the need for additional parks on the east end of the city.

Clark Park was most frequently cited as needing improvements, including:

- Playground equipment
- Safety and lighting improvements
- More benches
- More parking
- Upgraded sports fields and facilities, including new softball dugouts

A few respondents suggested upgrades to Fox Park, including improving the water feature (particularly drainage) and play equipment. Others suggested expanding or adding to the aquatic center, and some residents noted that the restrooms downtown need upgrades. There also was support for additional sports parks and improvements to existing sports fields.

Other suggestions included:

- Clean up the river corridor
- Community arts center
- Off-leash dog areas
- Basketball courts

Several respondents also expressed the opinion that the city should maintain and improve its existing facilities before acquiring new properties (see question #3).

**Please use the remaining space to make any additional comments about park, recreation and trail needs in the city that you would like staff to consider during the master planning process.**

Responses to this open-ended question tended to fall into one of several categories. These are listed below, with the number of responses for that category shown in parentheses.

- Provide safe trails/sidewalks for walking and biking, especially into town (12)
- Ensure that basic services like street repair are provided before spending more money on parks (8)
- Preserve open space as development of new homes occurs (5)
- Ensure safety of park users/adequate policing of parks and trails (5)
- Don't spend more money on parks and recreations (4)
- Maintain existing facilities rather than acquire and develop new parks (4)
- Provide programs and facilities for youth and families with children (4)
- Maintain restrooms (2)
- Need for a community center/sports complex with meeting space (2)
- Focus on larger parks (2)
- Address ATV use (2)
- Need for more sports fields (2)

Other ideas reported include:

- Equestrian trails near town
- ADA accessible trails
- Summer day camp
- Skating rink
- Rock climbing wall
- Mini golf
- Covered meeting areas

## Appendix G

### Trail Design Types

The following table provides a quick reference chart for the various types of trails and the accepted standards.

	Off-Street Trails			On-Street Trails			
	Shared Use Path	Natural Trail	Accessway	Sidewalk/pathway	Bicycle Lane	Shared Roadway	Accessway
Facility Type	Shared use path	Soft surface trail	Shared use path	Adjacent to roadway	Within roadway	Within roadway	Stairs, elevator, incline, bridge, alley, etc.
Users	bicyclists pedestrians wheelchairs baby strollers equestrians skaters	bicyclists** pedestrians  equestrians	bicyclists pedestrians wheelchairs*** baby strollers equestrians skaters	bicyclists pedestrians wheelchairs baby strollers  skaters	bicyclists pedestrians wheelchairs baby strollers  skaters	bicyclists pedestrians wheelchairs baby strollers  skaters	depends on facility type
Width	6' - 12'  2' soft shoulders	2' - 12'	7' 4' shoulders 5' in greenways	5' - 12'	5' - 6'	Depends on street width	depends on facility type
Surface	Paved or other smooth-rolling surface to accommodate all trail users	Earth, gravel, wood shavers, or other soft surface material	Concrete  Gravel in greenways	Concrete	Asphalt	Asphalt	depends on facility type

### Trail Designs

The following cross sections illustrate standard treatments for the primary trail design opportunities in Molalla. This section should be supplemented with other trail design documents, including ODOT's Bicycle and Pedestrian Master Plan, Metro's "Green Trails: Guidelines for Building Environmentally Friendly Trails," AASHTO, and the MUTCD.

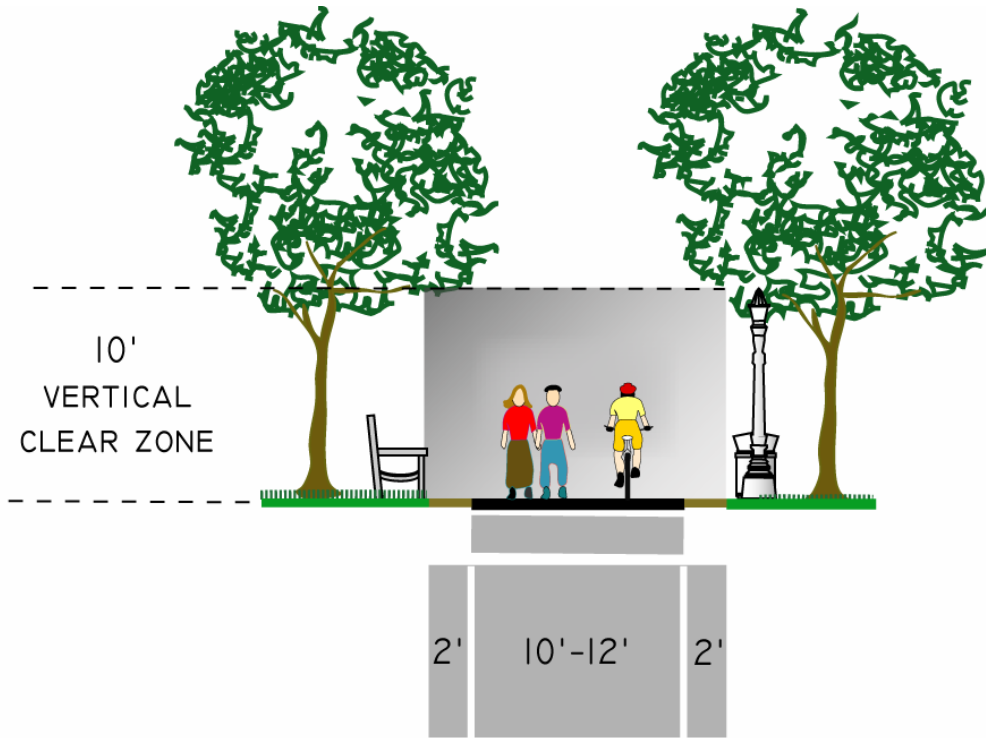


Figure 1. Shared Use Path

## Shared Use Path

Figure 3 illustrates a typical shared use path design that is appropriate for Molalla's backbone trails, some primary ring trails, and radial connectors. This trail is designed to accommodate two-way bicycle and pedestrian traffic, typically has its own right-of-way, and can accommodate maintenance and emergency vehicles. This type of trail is typically paved (asphalt or concrete) but can also be a surface that provides a smooth surface, as long as it meets ADA requirements. Wider soft shoulders should be provided for equestrians and runner/joggers if space allows.

# Shared Use Paths Paralleling High Volume/Speed Roadways

On roadways with 3,000 or more vehicles a day, bicycle lanes should be used to improve bicyclist safety and comfort. A buffer or curb must separate the shared use path or sidewalk from the roadway for pedestrian safety. The width of the bicycle lane, buffer, and sidewalk or path should appropriately reflect the volume and speed of the vehicles using the roadway. Roadways with higher traffic volumes and speeds should have wider bicycle and pedestrian facilities or greater separation.

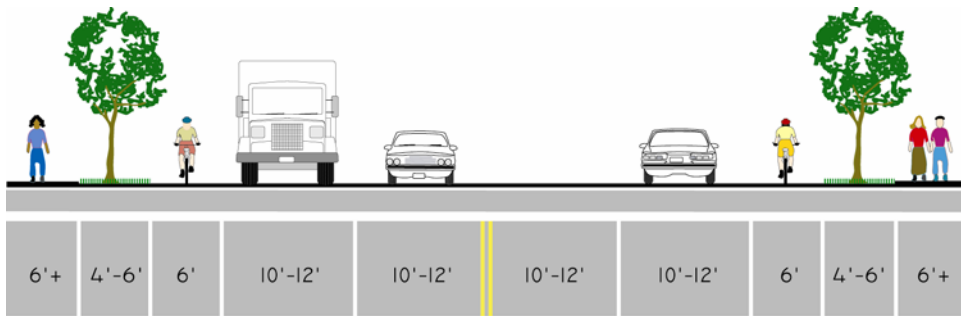


Figure 2. Shared Use Path on a High-Volume, High-Speed Roadway

Figure 4 illustrates typical bicycle and pedestrian trail accommodation in urbanized areas. The width of the sidewalk should depend on anticipated use; more users warrant a wider walkway. Sidewalks should be a minimum of 6' with a 4' minimum planter or 6' minimum tree well. Bicycle lanes should be 5' to 6'; 4' minimum is allowed under certain circumstances.

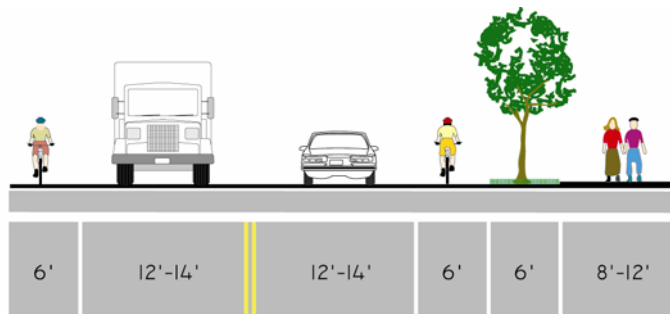


Figure 3. Shared Use Path Option 1 with Bike Lanes on a High-Volume, High-Speed Roadway

Some arterials and major collectors can accommodate a shared use path on one side of the roadway and on-street bicycle lanes for commuter bicyclists (*Figure 5*). The shared use path provides a comfortable walking space for pedestrians and enables children and recreational bicyclists to ride without the discomfort of riding in a busy street. This

configuration works best along roadways with limited driveway crossings and with services primarily located on one side of the roadway.

Sometimes a shared use path can provide trail accommodation on high-volume, high speed roadways (Figure 6). This type of trail works best in corridors where there are limited driveway/intersection crossings and few desirable destinations on the side of the roadway without the trail, like along Highway 213 or along local roadways with access management and minimal driveway use. The trail should be at least 8' wide (preferable 12') with a 6' or greater vegetated buffer.

*Note: This treatment should be used only after a detailed analysis of the corridor has been conducted by a registered engineer. Driveway/uncontrolled intersection crossings should not exceed 4 for each quarter-mile.*

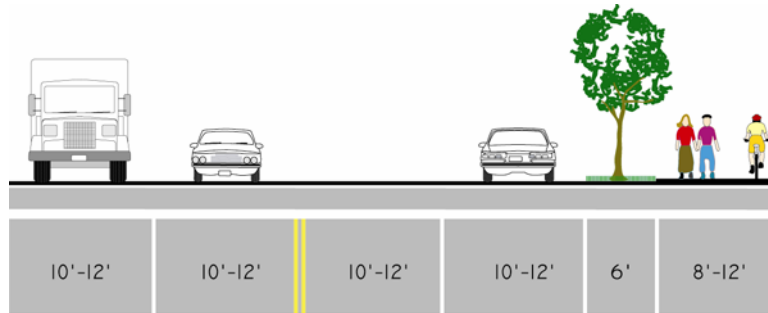


Figure 4. Shared Use Path Option 2 on a High-Volume, High Speed Roadway

## On-Street Facilities: Moderate Volume Roadways

Some urban roadways can accommodate bicyclists with a wide outside travel lane if there is no shoulder or insufficient space for a bicycle lane. The lane should be wider on roadways with steep grades where bicyclists need more maneuvering space. If space is constrained, the wider lane should be provided on the uphill side of the roadway.

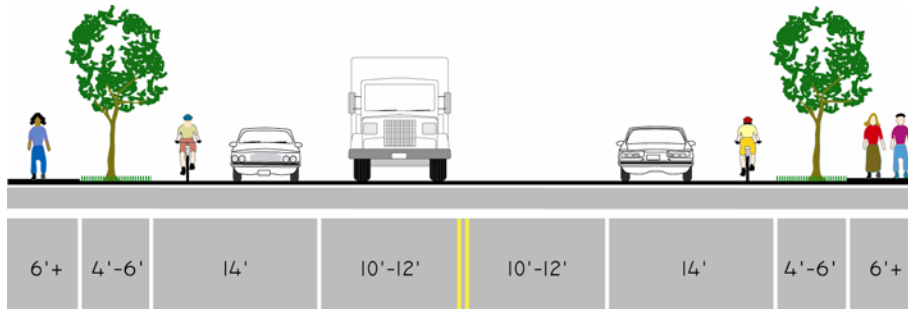


Figure 5. On-Street Facility (wide outside lanes) and a Sidewalk

## On-Street Facilities: Low Volume Roadways

On a low volume, low speed roadway (i.e., residential or neighborhood streets), many bicyclists can safely share the road with vehicles. Pedestrians should be separated from the roadway with a buffer or a curb. A curb must be present if there is insufficient space for a buffer. The width of the sidewalk or trail should depend on the traffic volume and speeds of the adjacent roadway.

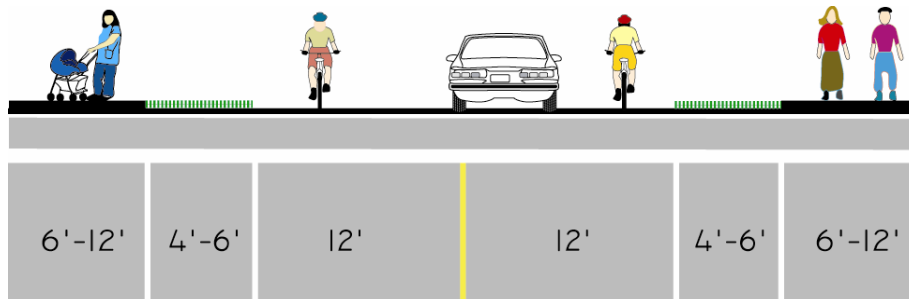


Figure 6. On-Street Facility on a Low Volume, Low Speed Roadway

## Micropaths: City Trails

City trails provide access for most, if not all, trail users within neighborhoods, parks, greenspaces, and other recreational areas. They are similar to regional trails in that they typically have their own right-of-way and serve only non-motorized users. These trails should be at least 6' wide and at least 8' wide if bicycle use is anticipated. All efforts should be made so that at least one ADA accessible trail is available and serves the most desirable parts of the area (i.e., picnic areas, viewpoints, playground equipment, etc.)

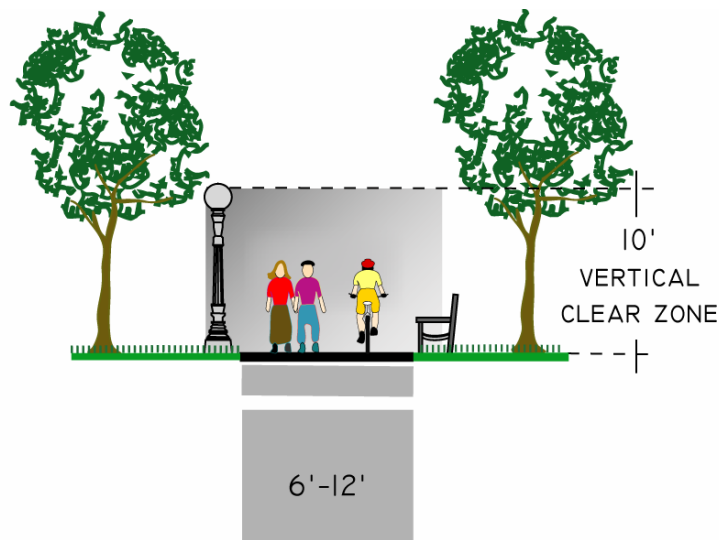


Figure 7. Paved City Trail

## Micropaths: Natural Trails

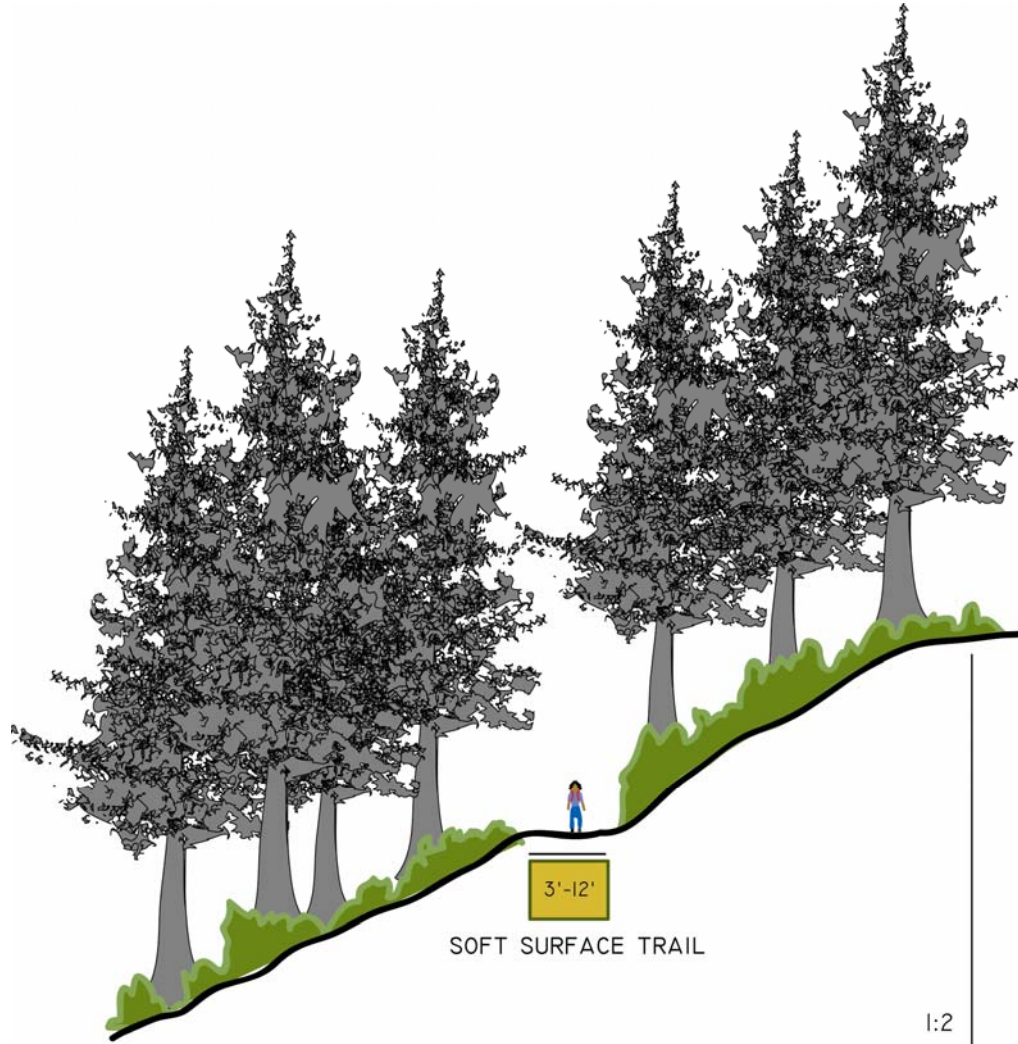


Figure 8. Natural Trail

Natural trails are usually considered when a trail is desired next to a natural resource. Trail width will vary depending on the existing topographic and environmental conditions. Natural trails should take into account issues like drainage, erosion, compaction/impaction from anticipated use, presence of waterways and sensitive riparian areas, habitat areas, environmental guidelines, such as “Green Trails” Guidelines for Environmentally Friendly Trails” by Metro, and regulations.

Trail width will depend on intended users. For example, narrower widths should be used in environmentally constrained areas with only hiking uses intended. Wider widths are desirable for shared bicycle and/or equestrian use. Areas with natural trails (i.e., natural parks and greenspaces) should have a complimentary accessible route that meets or exceeds ADA standards in addition to the natural trails.

## Micropaths: Accessways

Accessways provide direct connections for trail users to schools, parks, community centers, retail areas, neighborhoods, and other trails. They are intended to be short, direct connections to reduce unnecessary out-of-direction travel for bicyclists and pedestrians. The accessway should not exceed 5% slope to accommodate all users. Accessways in parks, greenways, or other natural resource areas may have a 5' wide gravel path with wooden, brick or concrete edgings. Accessways may have more innovative materials such as a boardwalk (which may be more appropriate in a natural area).

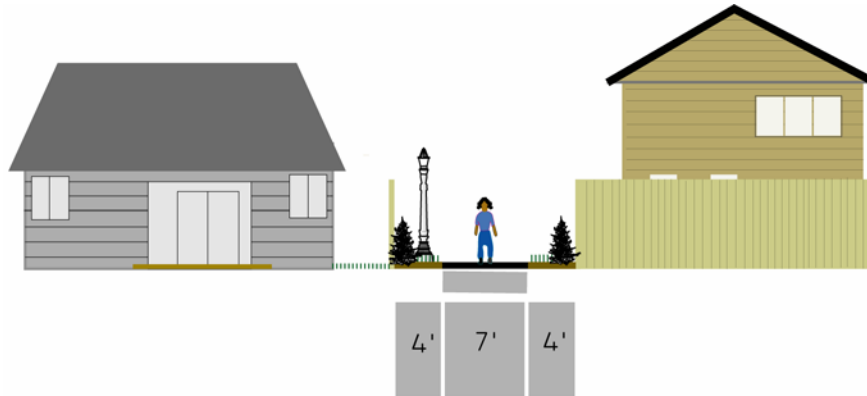


Figure 9. Bicycle and Pedestrian Accessway



Boardwalk bridge

### ***Trail-Roadway Crossings***

Like most trails in built urban areas, Molalla's trails must cross roadways at certain points. These roadway crossings may be designed at-, below-, or above-grade. At-grade crossings create a potentially high level of conflict between trail users and motorists. However, well-designed crossings have not historically posed a safety problem, as evidenced by the thousands of successful trails around the United States with at-grade crossings. Designing safe grade crossings is a key to safe implementation of this Plan.

Trail-roadway crossings should comply with the AASHTO (American Association of State Highway and Transportation Officials), ODOT (Oregon Department of Transportation), and MUTCD (Manual on Uniform Traffic Control Devices) standards.

In some cases, a required trail crossing may be so dangerous or expensive (e.g., to build an undercrossing or overcrossing) as to affect the feasibility of the entire alignment. However, in most cases, trail crossings can be properly designed at-grade to a reasonable degree of safety and to meet existing traffic and safety standards. No grade-separated crossings are anticipated in this Molalla Trails Plan.

Evaluation of trail crossings involves analysis of vehicular and trail user traffic pattern, including speeds, street width, traffic volumes (average daily traffic, peak hour traffic), line of sight, and trail user profile (age distribution, destinations). This plan identifies the most appropriate crossing options given available information, which must be verified and/or refined through the actual engineering and construction document stage.

## Basic Crossing Prototypes

The proposed intersection approach in this plan is based on established standards, published technical reports, and the experiences from existing facilities. Virtually all crossings fit into one of four basic categories:

- *Type 1: Unprotected/Marked*  
Unprotected/marked crossings include trail crossings of residential, collector, and sometimes major arterial streets or railroad tracks.
- *Type 2: Route Users to Existing Intersection*  
Trails that emerge near existing intersections may be routed to these locations, provided that sufficient protection is provided at the existing intersection.
- *Type 3: Signalized/Controlled*  
Trail crossings that require signals or other control measures due to traffic volumes, speeds, and trail usage.

## Type 1: Unprotected/Marked Crossings

An unprotected crossing (Type 1) consists of a crosswalk, signing and often no other devices to slow or stop traffic. The approach to designing crossings at mid-block locations depends on an evaluation of vehicular traffic, line of sight, trail traffic, use patterns, vehicle speed, road type and width and other safety issues such as the proximity of schools. The following thresholds outlined recommend where unprotected crossings may be acceptable:

- Install crosswalks at all trail-roadway crossings
- Maximum traffic volumes:
  - $\leq 9,000$ -15,000 ADT
  - up to 15,000 ADT on two-lane roads, preferably with a median.
  - up to 12,000 ADT on four-lane roads with median.
- Maximum travel speed
  - 35 mi/h



**Raised Crosswalk**

- Minimum line of sight:
  - 25 mi/h zone: 155 feet
  - 35 mi/h zone: 250 feet
  - 45 mi/h zone: 360 feet

On two lane residential and collector roads below 15,000 ADT with average vehicle speeds of 35 mi/h or less, crosswalks and warning signs (“Bike Xing”) should be provided to warn motorists, stop signs and slowing techniques (bollards/geometry) should be used on the trail approach. Care should be taken to keep vegetation and other obstacles out of the sight line for motorists and trail users. Engineering studies should be done to determine the appropriate level of traffic control and design. On

roadways with low to moderate volumes of traffic (< 12,000 ADT) and a need to control traffic speeds, a raised crosswalk may be the most appropriate crossing design to improve pedestrian visibility and safety.

The crosswalks are raised 150 mm above the roadway pavement, similar to speed humps, to an elevation that matches the adjacent sidewalk. The top of the crosswalk is flat and typically made of asphalt, patterned concrete, or brick pavers. Brick or unit pavers should be discouraged because of potential problems related to pedestrians, bicycles and ADA requirements for a continuous, smooth, vibration-free surface. Tactile treatments are needed at the sidewalk/street boundary so that visually impaired pedestrians can identify the edge of the street. Costs can range from \$5,000 to \$20,000 per crosswalk, depending on the width of the street, the drainage improvements affected, and the materials used for construction.

A flashing yellow beacon costing between \$15,000 and \$30,000, may be used, preferably one that is activated by the trail user rather than operating continuously. Some jurisdictions have successfully used a flashing beacon activated by motion detectors on the trail, triggering the beacon as trail users approach the intersection. This equipment, while slightly more expensive, helps keep motorists alert.



**Type 1 Crossing**

Crossings of higher volume arterials over 15,000 ADT may be unprotected in some circumstances – for example, if they have 85<sup>th</sup> percentile speeds of 30 mi/h or less and have only two lanes of traffic. Such crossings

would not be appropriate, however, if a significant number of school children used the trail.

## Type 2: Route Users to Existing Intersection

Crossings within 250 feet of an existing signalized intersection with pedestrian crosswalks are typically diverted to the signalized intersection for safety purposes. For this option to be effective, barriers and signing may be needed to direct trail users to the signalized crossings. In most cases, signal modifications would be made to add pedestrian detection and to comply with the ADA. In many cases, such as on most community trails parallel to roadways, crossings are simply part of the existing intersection and are not a significant problem for trail users.

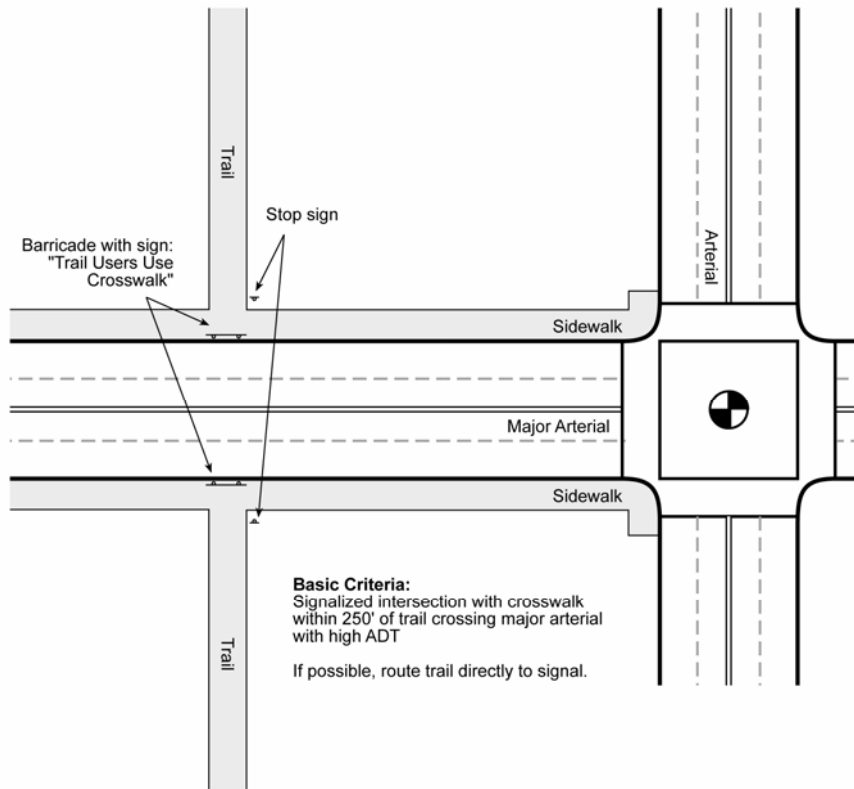


Figure 10. Type 2 Roadway Crossing Schematic

## Type 3: Signalized/Controlled Crossings

New signalized crossings are recommended for crossings more than 250 feet from an existing signalized intersection and where 85th percentile travel speeds are 40 mi/h and above and/or ADT exceeds 15,000 vehicles. Each crossing, regardless of traffic speed or volume, requires additional review by a registered engineer to identify sight lines, potential impacts on traffic progression, timing with adjacent signals, capacity and safety.

Trail signals are normally activated by push buttons, but also may be triggered by motion detectors. The maximum delay for activation of the signal should be two minutes, with minimum crossing times determined by the width of the street. The signals may rest on

flashing yellow or green for motorists when not activated, and should be supplemented by standard advanced warning signs. Typical costs for a signalized crossing range from \$150,000 to \$250,000.

## Signing and Striping

Crossing features for all roadways include warning signs both for vehicles and trail users. The type, location, and other criteria are identified in the Manual for Uniform Traffic Control Devices (MUTCD). Adequate warning distance is based on vehicle speeds and line of sight. Signage should be highly visible; catching the attention of motorists accustomed to roadway signs may require additional alerting devices such as a flashing light, roadway striping or changes in pavement texture. Signing for trail users must include a standard stop sign and pavement marking, sometimes combined with other features such as bollards or a kink in the trail to slow bicyclists. Care must be taken not to place too many signs at crossings lest they overwhelm the user and lose their impact.

Directional signing may be useful for trail users and motorists alike. For motorists, a sign reading “Bicycle Trail Xing” along with a Molalla trail emblem or logo helps both warn and promote use of the trail itself. For trail users, directional signs and street names at crossings help direct people to their destinations.

The directional signing should impart a unique theme so trail users know which trail they are following and where it goes. The theme can be conveyed in a variety of ways: engraved stone, medallions, bollards, and mile markers. A central information installation at trailheads and major crossroads also helps users find their way and acknowledge the rules of the trail. They are also useful for interpretive education about plant and animal life, ecosystems, and local history.

A number of striping patterns have emerged over the years to delineate trail crossings. A median stripe on the trail approach will help to organize and warn trail users. The actual crosswalk striping is a matter of local and State preference, and may be accompanied by pavement treatments to help warn and slow motorists. The effectiveness of crosswalk striping is highly related to local customs and regulations. In communities where motorists do not typically yield to pedestrians in crosswalks, additional measures may be required.

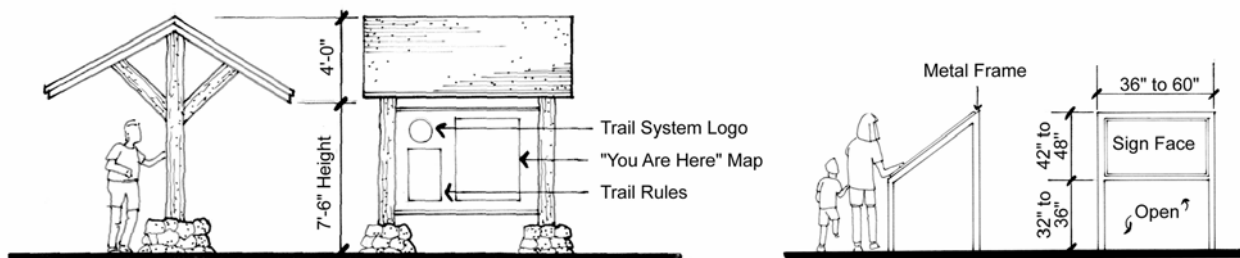


Figure 11. Trailhead information installation examples



Sample informational and directional signage



Wooden bollard with directional information



Inlaid medallions



Stone mileage marker

## Trail Features

There are a number of amenities that make a trail inviting to the user. Below are some common items that make trail systems stand out.



### Interpretive Installations

Interpretive installations and signs can enhance the trail experience by providing information about the history of Molalla. Installations can also discuss local ecology, environmental concerns, and other educational information.



### Water Fountains and Bicycle Parking

Water fountains provide water for people (and pets, in some cases) and bicycle racks allow trail users to safely park their bikes if they wish to stop along the way, particularly at parks and other desirable destinations.



### Pedestrian-Scale Lighting and Furniture

Pedestrian-scale lighting improves safety and enables the trail to be used year-round. It also enhances the aesthetic of the trail. Lighting fixtures should be consistent with other light fixtures in the city, possibly emulating a historic theme.

Providing benches at key rest areas and viewpoints encourages people of all ages to use the trail by ensuring that they have a place to rest along the way. Benches can be simple (e.g., wood slates) or more ornate (e.g., stone, wrought iron, concrete).



### Maps and Signage

A comprehensive signing system makes a trail system stand out. Informational kiosks with maps at trailheads and other pedestrian generators can provide enough information for someone to use the trail system with little introduction – perfect for areas with high out-of-area visitation rates as well as the local citizens.



### Art Installations

Local artists can be commissioned to provide art for the trail system, making it uniquely distinct. Many trail art installations are functional as well as aesthetic, as they may provide places to sit and play on.

## Appendix H

ID	Trail	From - To	Type	Description	Other	Crossings	Priority
<b>OFF-STREET TRAILS</b>							
1	Molalla Rail Trail	South Molalla Rd. – Molalla Forest Rd.	Shared use path	Rail-trail would form important north-south trail connection through the heart of Molalla, following existing rail alignment. Long-term possibility to connect north to Canby.		Type 1, Type 3	1
2	Bear Creek Greenway	Molalla Forest Rd – Molalla City Limits	Shared use path	Trail would follow Bear Creek east-west through town, to meet up with equestrian trail on east side of town. Possible future extension southeast of town.	Likely wetland issues to be resolved in trail planning. Boardwalk may be necessary for parts.	Type 1	1
3	Land Lab Trail	Molalla High School – Molalla HS Land Lab (on Vaughan Rd.)	Nature trail	Nature trail would connect directly from High School to off-site 50 acre land lab site.		None	1
4	Buckeroo to Land Lab	Molalla Buckeroo (Shirley St.) – Molalla HS Land Lab (Vaughan Rd.)	Nature trail	Nature trail would connect Vaughan Rd. trail and HS Land Lab with north-south equestrian trail.	Should be designed for equestrian use.	None	2
5	Buckeroo Equestrian Trail	Molalla Buckeroo (Shirley St.) – Molalla Forest Rd.	Nature trail	Trail would allow users, including equestrians, to traverse from north to south end of town in rural setting. Short segment will be parallel to Shirley St.	Should be designed for equestrian use.	Type 1	2
6	Bear Creek West Trail	OR 213 – west of water treatment plant	Shared use path	Trail will allow users to access rural areas to west of town, including water treatment plant.	Western terminus should be determined during future planning.	Type 1	2
7	Water Treatment Trails	Loop around water treatment plant, connecting to Bear Creek West Trail	Nature trail	Trail will allow a scenic loop around water treatment plant.	Trail will need to be designed to balance trail access with security needs.	None.	2

ID	Trail	From - To	Type	Description	Other	Crossings	Priority
8	OR 211 to Toliver Connector	Toliver Rd. to OR 211	Nature trail	Nature trail will connect school facilities with trail loop south of OR 211	Passes through city-owned land.	Type 1	2

## ON-STREET TRAILS

9	Molalla Rail Trail to High School Connector	Molalla Rail Trail – Molalla High School (via Miller & Frances)	Shared roadway and sidewalks	Connects rail-trail with High School, Clark Park, Aquatic Center. and Buckeroo Stadium. Shared roadway for bicycles and sidewalks for pedestrians.		Type 1	1
10	Cole Avenue	Molalla High School (Frances St.) – Molalla Forest Rd.	Bike lanes, sidewalks, shared use path	Bike lanes and sidewalks from Molalla High School to Fifth St; Shared use path from Fifth to Molalla Forest Rd.	Possible wetland issues to be researched in future planning.	Type 1	1
11	Molalla Forest Road: OR 211 to Bear Creek Connector	OR 211 to Bear Creek	Bike lanes and pathway	Finishes outer ring trail network by connecting OR 211 with trails to south of town (Bear Creek Greenway and Molalla Forest Rd.)	Project defined separate from Molalla Forest road as near-term priority because it completes trail loop.	Type 1	1
12	Toliver Road	Molalla River Middle School – Molalla Rail Trail	Bike lanes and sidewalks	Bike lanes and sidewalks from rail-trail to schools via Toliver	Bike lanes and sidewalks are present in some parts.	None	1
13	Fifth Street	Molalla Rail Trail – Cole Rd.	Bike lanes and sidewalks	Bike lanes and sidewalks from proposed rail-trail to Cole Rd. trails	Bike lanes and sidewalks are present in some parts	Type 1, Type 2	1
14	OR 213	Vick Rd. – OR 211	Bike lanes and sidewalks	Bike lanes and sidewalk along Highway 213 from Vick Rd. to OR 211	Sidewalks are present in some parts	Type 2	2
15	Vick Road	OR 213 – Molalla Rd.	Shared roadway and sidewalks	Shared roadway and sidewalks along Vick Rd. from OR 213 to Molalla Rd.	If development patterns increase traffic volume more than anticipated, bike lanes may be more appropriate.	Type 1	2
16	Vick to Vaughan Connector	Vick Rd. – Vaughan Rd	Bike lanes and pathway	Bike lanes and adjacent pathway for short segment on Molalla, connecting trails on Vick & Vaughan.	If development patterns increase traffic volume more than anticipated, bike	Type 1	2

ID	Trail	From - To	Type	Description	Other	Crossings	Priority
					lanes may be more appropriate.		
17	Vaughan Rd.	Molalla Rd. – Land Lab	Bike lanes and pathway	Bike lanes and adjacent pathway along Vick Rd. Pathway may be designed for equestrian use if that is desirable at time of implementation.	If development patterns increase traffic volume more than anticipated, bike lanes may be more appropriate.	Type 1	2
18	Molalla Forest Rd.	OR 211 – Buckeroo Equestrian Trail	Bike lanes and pathway	Bike lanes and adjacent pathway along Molalla Forest Rd.	If development patterns increase traffic volume more than anticipated, bike lanes may be more appropriate.	Type 2	2
19	OR 211	OR 213 – Molalla Rail Trail	Bike lanes and sidewalks	Bike lanes and sidewalks from OR 213 to Molalla Rail Trail.	Already in OR 211 plan.	Type 2	2
20	Leroy Avenue	Toliver – OR 211	Bike lanes and sidewalks	Bike lanes and sidewalks from Toliver to OR 211.		Type 2	2

## Appendix I

**Table 1. Funding Sources for Park, Recreation, and Trail Projects**

Source	Description	Eligible Projects	Funding Cycle
Recreational Trails Grants	Coordinated by Oregon State Parks. Funds can be used for ROW acquisition and construction.	Off-Street Trails	Annual
Land and Water Conservation Fund (LWCF)	Federal funds coordinated by Oregon State Parks. for planning and acquiring outdoor recreation areas and facilities, including trails. Funds can be used for ROW acquisition and construction.	Recreation areas and facilities; off-Street Trails	Annual
Measure 66 funds from Oregon State Lottery	Coordinated by Oregon State Parks. Funds can be used for acquisition and construction.	Development and protection of parks and natural resources, Off-Street Trails	2 years
Transportation Enhancements	Administered by Oregon Department of Transportation (ODOT). Must serve transportation need.	Off- and On-Street Trails	2 years
Oregon Bike/Ped Grants	Administered by ODOT's Bicycle and Pedestrian Program. Must be in public ROW.	On-Street Trails	2 years
System Development Charges (SDCs)	Fees on new construction allocated for parks, streets, and public improvements. Funds can be used for land and ROW acquisition, park planning and development, and trail construction.	All Park and Trail Types	Varies
Local/Regional bond measures	Funds can be used for land or ROW acquisition, engineering, design and trail construction.	All Trail Types	Varies
Tax Increment Financing/Urban Renewal Funds	Park or part of trail project must be located in an urban renewal district which meets certain economic criteria and is approved by a local governing body.	All Park and Trail Types	Varies
Local Traffic Safety Commission	Funding for street crossings and signals.	All Trail Crossings	Varies
Safe Routes to School Funds	Federal funds for pedestrian and bicycle facilities to improve school safety	All Trail Types	
Congestion Mitigation and Air Quality (CMAQ) funds	Federal funding for bicycle and pedestrian facilities that reduce travel by automobile. Recreational facilities generally are not funded.	All Trails	2 years

## Appendix J

### ***Maintenance Guidelines***

The following table summarizes a recommended maintenance schedule for the Molalla Trail system. These guidelines address maintenance on the off-street portions of the trails. On-street portions should be maintained per the standards of the responsible jurisdiction.

**Table 2. Maintenance Tasks and Frequency of Need**

Item	Frequency
Inspections	Seasonal - at both beginning and end of summer
Signage Replacement	1 - 3 years
Pavement Markings Replacement	1 - 3 years
Major damage response (fallen trees, washouts, flooding)	Schedule based on priorities
Pavement Sealing, Potholes	5 - 15 years
Introduced tree and shrub plantings, trimming	Every 1- 3 years
Culvert Inspection	Before winter and after major storms
Cleaning Ditches	As needed
Trash Disposal	Weekly during high use; twice monthly during low use
Lighting Luminaire Repair	Once a year
Pavement Sweeping/Blowing	As needed, before high use season. Weekly in fall.
Maintaining culvert inlets	Inspect before the onset of the wet season, then again in early fall
Shoulder plant trimming (weeds, trees, brambles)	Twice a year: middle of growing season and early fall
Waterbar maintenance (earthen trails)	Annually
Site furnishings, replace damaged components	As needed
Graffiti Removal	Weekly, as needed
Fencing Repair	Inspect monthly for holes and damage, repair immediately
Shrub/Tree Irrigation for introduced planting areas	Weekly during summer months until plants are established
Litter Pick-up	Weekly for high use; twice a month for low use

## Appendix K